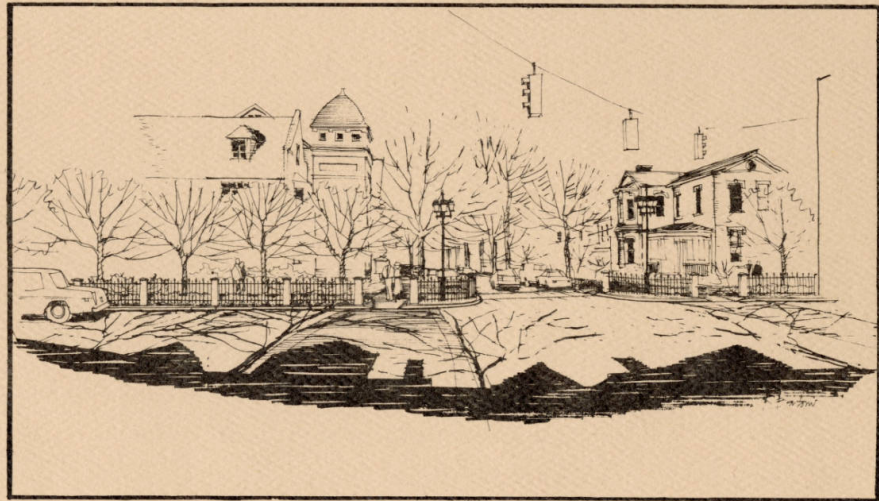


URBAN DESIGN SECTION COPY

DO NOT REMOVE

High Street Development Standards: South High

Livingston To Greenlawn Avenue



Columbus, Ohio August 1979

Columbus, Ohio

PREPARED FOR:

THE DEPARTMENT OF DEVELOPMENT
CITY OF COLUMBUS, OHIO

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1 Intent of the Study

The High Street Study was initiated in 1977 by the Department of Development to identify improvements for Columbus' north/south artery between Fifth Avenue and Greenlawn Avenue. The study incorporates the comments and suggestions from public officials and private citizens concerned with the future of the street and its vital role in the economic life of the community. The intent of the study was to develop consensus on a plan for improving the street and adjacent properties, including recommendations on funding, capital improvement budgets and implementation steps.

The five-mile long study area was divided into three districts based on geographic and community characteristics. This report documents the recommendations related to the segment of High Street in the south section between Fulton Street and Greenlawn Avenue.

2 Process

The High Street Study reviewed a range of technical issues and community concerns. During the course of the study, the Department of Development and its consultants have worked with the South High Task Force, the Study Review Committee, and with City agencies to arrive at a plan for High Street which addressed the concerns of each group. Specifically, the study included both public meetings and technical work-sessions with the following groups:

1. The South High Street Task Force: This group was assembled by the Department of Development at the beginning of the study, and consisted of South High Street businessmen, property owners, and representatives of the community organizations in the area. The charge to this Task Force consisted of three assignments:

- a) to assist in identifying the issues and problems of the study area, and review in detail the consultant's design alternatives;
- b) to provide communication between city government and area interest groups; and
- c) to be public advocates for improvements to High Street supported by the Task Force.

The Task Force has pursued these assignments over the course of the study primarily through public meetings coordinated with the Department of Development. These Task Force meetings have provided a forum for an exchange of views by private citizens as well as public officials, and will

continue to be the most effective way for resolving the many important issues which will arise as the project proceeds into more detailed phases.

2. The Study Review Committee: The Study Review Committee was assembled to provide an overview of the three study areas involved in the High Street study (north, downtown and south), and to provide coordination with various public agencies responsible for implementing the study recommendations.

3. City Agencies: Various city agencies participated in the study through work sessions with consultants and through the Task Force meetings. Specifically, the following agencies contributed to the recommendations found in this report:

- a) Department of Development, representing the overall planning and urban-design concerns for the street;
- b) Division of Traffic Engineering, providing information on overall traffic planning for the downtown, and reviewing specific transit/traffic alternatives for the High Street corridor;
- c) Division of Engineering, providing information on existing conditions, utilities, construction standards, and cost issues;
- d) Division of Electricity, coordinating lighting guidelines;
- e) Division of Parks and Recreation, providing information on street tree-plantings in the public right-of-way.

4. Central Ohio Transportation Authority (COTA): COTA provided guidance and review related to the transit operations on High Street, and particularly future operations in terms of bus volumes, re-routings, express bus service, and passenger conveniences.

The High Street study involved an exchange of ideas among community groups, city agencies and the city's consultants on a wide range of issues affecting the south study area such as traffic, transit, pedestrian circulation, private and public improvement activities, public safety, funding and environmental quality. The issues were identified by the Task Force; these issues were analyzed by the consultants and city agencies, and recommendations were made to the Task Force for their review and comment.

The issues were, by necessity, approached on a priority basis with the resolution of vehicular circulation needs within the public right-of-way being the first issue to resolve because of the critical need of traffic to occupy a major portion of the right-of-way. The second issue was parking for businesses on the street.

The environmental issues and private redevelopment opportunities which supported the commercial nature and character of the street were assessed. This report documents the recommendations for environmental improvements to High Street which support the goals as stated by the Task Force.

The South High Task Force Members easily and rapidly identified the issues which concerned the community, and were united in their response to the consultant's recommendations. The level of participation of the Task Force remained high throughout the study, and provided the beginning of the broad based support which will be necessary to implement the recommendations of the Task Force and the consultants.

To transmit the recommendations of the Task Force to the community-at-large, the consultants with the Department of Development published the High Street "Consensus", a news-bulletin which described the process, deliberations and recommendations of the study.

Funding, cost and implementation issues directly reflect the planning and design decisions reached during the study process. These recommendations are, in many respects, the most critical elements of the report in that the source of funding and the vehicle or structure for managing the implementation process are critical to the ultimate success of the High Street improvements.

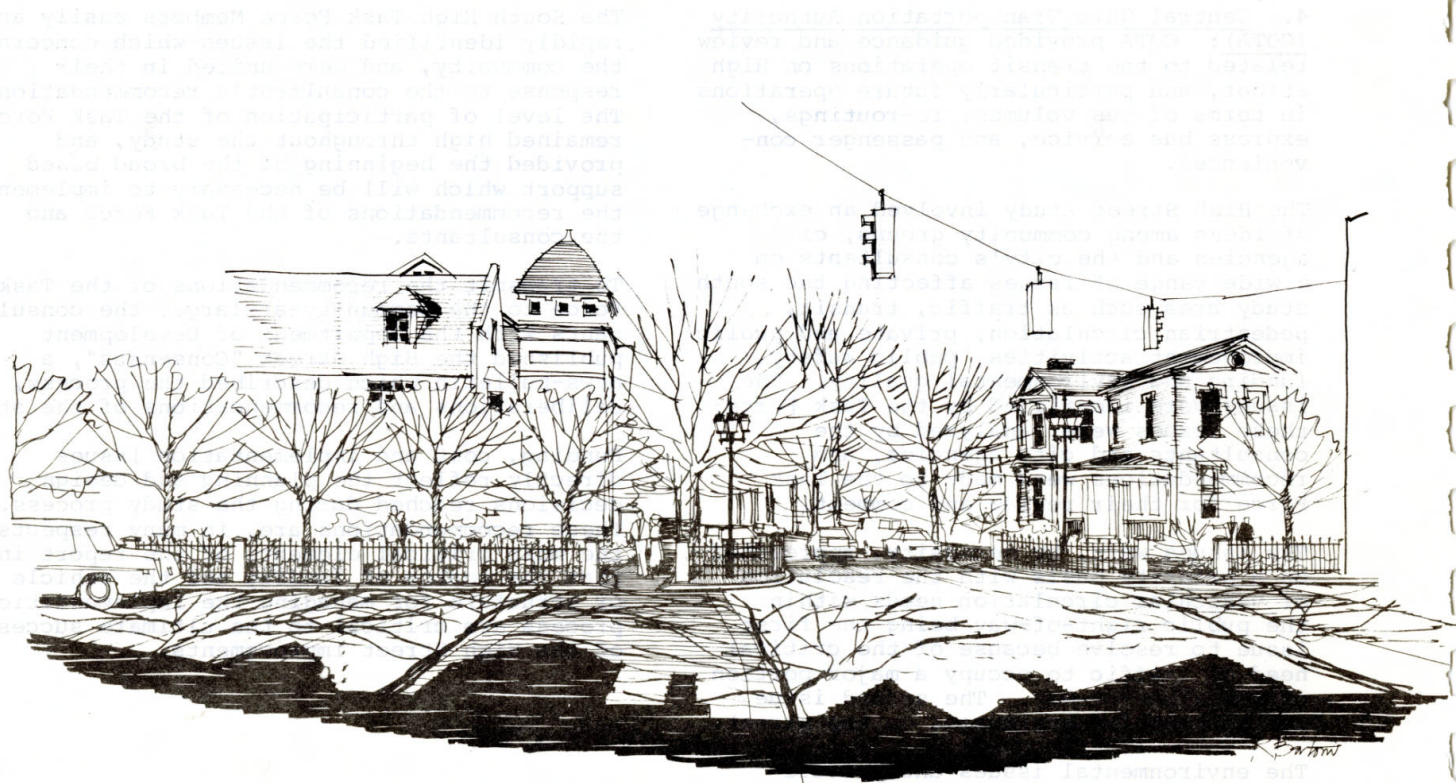


Figure 1 View of Proposed Entrance to German Village

3 Urban Design Concept

The south section of High Street originally developed as a residential area with some local retail, and in the blocks immediately south of Livingston Avenue, there was a mixture of light industry and warehousing. The pre-dominate manufacturing-use of the district was the production of malt-beverages in the brewery area located on Front Street. The area adjacent to the street was intensely developed for residential use.

The area today retains much of the physical character of the early 1900's; however, the mixture of business-uses and residential composition of the area has changed as a result of the successful renovation of the German Village area and the loss of manufacturing uses.

Residential use of properties along High Street today is limited to a few structures, and the old manufacturing structures in the area have been converted to wholesale or warehousing operations, or removed for surface parking development.

The purpose of developing an urban-design plan and development standards for the South High Street area is:

- * To improve the quality of the environment.

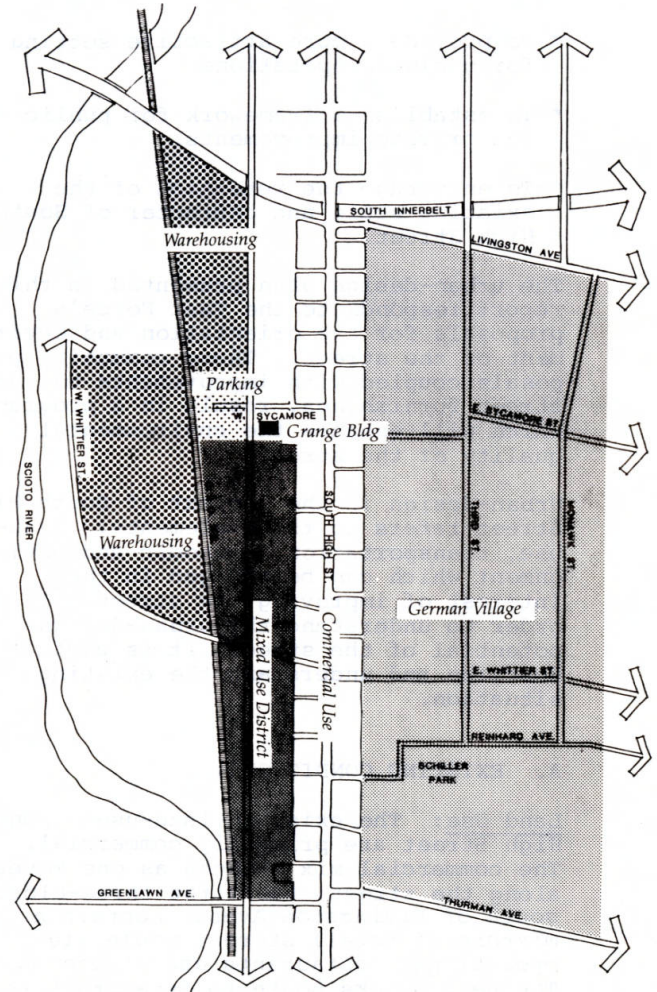


Figure 2 Existing Conditions

- * To provide a more attractive setting for business operations.
- * To establish a framework for public and private improvements.
- * To encourage the retention of the existing scale and character of South High Street.

The urban-design plan presented in this report responds to the Task Force's proposals for the orientation and environment of the street. These community proposals coupled with improvements to the street furnishings, result in a program which will enhance the environmental quality of the street.

Urban-design in the context of South High Street refers to these elements: land-use, transportation, economic and environment which can be managed in the interest of improving the street. In order to understand the urban-design potential of the street, it is useful to describe and understand the existing situation.

A. EXISTING CONDITIONS:

Land Use: The existing land-uses along High Street are primarily commercial. The commercial mix changes as one moves along the street; the first three-blocks south of Livingston Avenue contain a mixture of retail stores, wholesale operations, restaurants and office space. The next blocks south to Sycamore Street contain a mixture of institutional and office uses.

These first two-areas are intensively developed with buildings located on the property lines and generally occupying the entire street frontage. From Sycamore Street south, the land-use is predominately commercial office space, except for a cluster of fast-food operations and a private club.

This area is less-intensively developed as the commercial functions are carried-out for most operations in converted residential homes reflecting the recent shift of land-uses from residential to commercial office space through renovation of existing buildings. This rather rapid change in land-use has left the existing facade of the street intact. Changes which have occurred to the street facade have been located primarily in the north section of the district, and to-date, have respected the scale and character of the street. The result in this change in land-use patterns has had a positive impact on the street's image as a commercial district.

Immediately adjacent to High Street's east side is German Village which represents a successful residential redevelopment of an older residential neighborhood. On the west side of the street, a narrow strip of land between the Scioto River and the street contains a mixture of light manufacturing, wholesale, commercial and residential uses.

Transportation: South High Street in the study area, is a major arterial street serving traffic moving into and

out of the downtown area of Columbus. The majority of traffic on the street consists of commuters who are passing through the district and do not use the commercial establishments located on the street. The commercial activities on the street are such that they do not generate a high-volume of vehicular traffic throughout the day.

Building and commercial service access frequently takes place from the street although there is a system of parallel streets to the rear of commercial properties along High Street. These alleys are narrow, and the intensive use of the streets in the north section of the district for parking, sometimes makes it difficult to service some of the commercial buildings.

Parking is allowed curbside, and generally serves the businesses located on the street. As the working population of the area increased resulting from the shift to more commercial office space uses, private parking lots have been developed by those businesses moving into the area.

There is little pedestrian movement on the street throughout the day. The activity is generally confined to people arriving by automobile to do business with commercial establishments on the street. Public transportation to the district is provided by COTA with its bus routes on High and Whittier Streets. A more detailed discussion of traffic problems and recommendations is presented in Section 6.



Figure 3 View of High Street Today

Economic Outlook: In recent years, the economic outlook for properties along High Street has improved dramatically. This change is the result of a number of actions in the area which increased the attractiveness of the area as a place to do business, and accelerated the conversion of the street from its original residential-use to commercial office-use. Today, that conversion is almost complete and demands for office space exceeds the capacity of the street resulting in a trend to more intensive land-use than previously existed in the district.

Two potential redevelopment sites were identified along the street during the early period of this study. These sites presently have projects in planning or under construction. One of these sites

is located on the east side of High St. between Beck and Willow (see Figure 5). The other site identified as having a strong redevelopment potential is the southeast corner of Deshler and High St. (see Figure 10).

The street has developed a market orientation which is aimed at professional and service organizations. The growing number of legal offices and the location of the Capital University Law School suggests that the district in conjunction with the county-court complex will become the legal center of Columbus.

Environment: The existing environmental character of the street is relatively pleasant. The scale of buildings is consistent throughout the district, and the new projects which have been constructed to-date have respected the existing street scale. The use of construction materials is generally consistent, being predominately brick.

South of Sycamore Street, the buildings are set-back with front yards planted as lawns, giving the area a unique character which does not exist in the other study areas. The area in which the environmental quality needs upgrading is the organization of street furnishings within the right-of-way. The upgrading of this area will enhance what is presently a pleasant street scale.

B. URBAN DESIGN PROGRAM:

The urban-design program provides a framework for a series of joint public/private programs to improve the environment along High Street. The objective is to consider the whole corridor as a single space and that all improvements should be integrated into and enhance the urban-design program. The focus of the program is modest improvement programs for the public right-of-way, coupled with standards for private improvements.

Design Objectives: The program for improvements should attempt to:

- * Provide a better environment within the right-of-way.
- * Provide entrance points to German Village.
- * Provide for traffic improvements in the district.
- * Provide standards for private development along the street.
- * Have private development along the street contribute to the street's enhancement.
- * Encourage the retention of existing buildings in scale and character.



Figure 4 View of Proposed Tree Lawn for High Street

There are several special features which have been suggested to unify the appearance of the street and to identify points of access to German Village. The first feature is the reintroduction of the tree lawn in the northern part of the district. This would capitalize on a street feature which presently exists on approximately 2/3 of the street frontage, and would provide an element which would be consistent from Greenlawn Avenue to Livingston Avenue.

The existing tree lawn in the southern part of the district should be replanted where required to provide a uniform pattern of shade trees. The establishment of the tree lawn would unify the curb edge and sidewalk detail throughout the district.

The second feature identified is the emphasis on Stewart and Frankfort Streets as access points to German Village. These streets would receive special treatment including relocating the High Street curb approximately six feet into the existing right-of-way.

The two intersections would receive special treatment including brick sidewalks, an ornamental iron fence, ornamental street lights and special street planting. These features would be continued on Frankfort and Stewart Streets into German Village.

The following summary of improvements along the corridor is organized in terms of public and private actions.

Livingston Avenue to West Sycamore:

This district is the most intensively developed area on the street. The existing fabric of buildings along both sides of the street is almost intact, and those new buildings have generally respected the scale and character of the street. Continued renovation of existing structures should be encouraged.

Public action summary:

- * Re-introduction of tree lawn.
- * Sidewalk improvements where necessary.
- * Introduction of one-way street system on Blenkner, Hoster, Beck and Willow Streets.

Private action summary:

- * Private rehabilitation/redevelopment of properties not yet improved.
- * Redevelopment of block between East Beck and East Willow Streets.
- * Screen parking lots from streets.

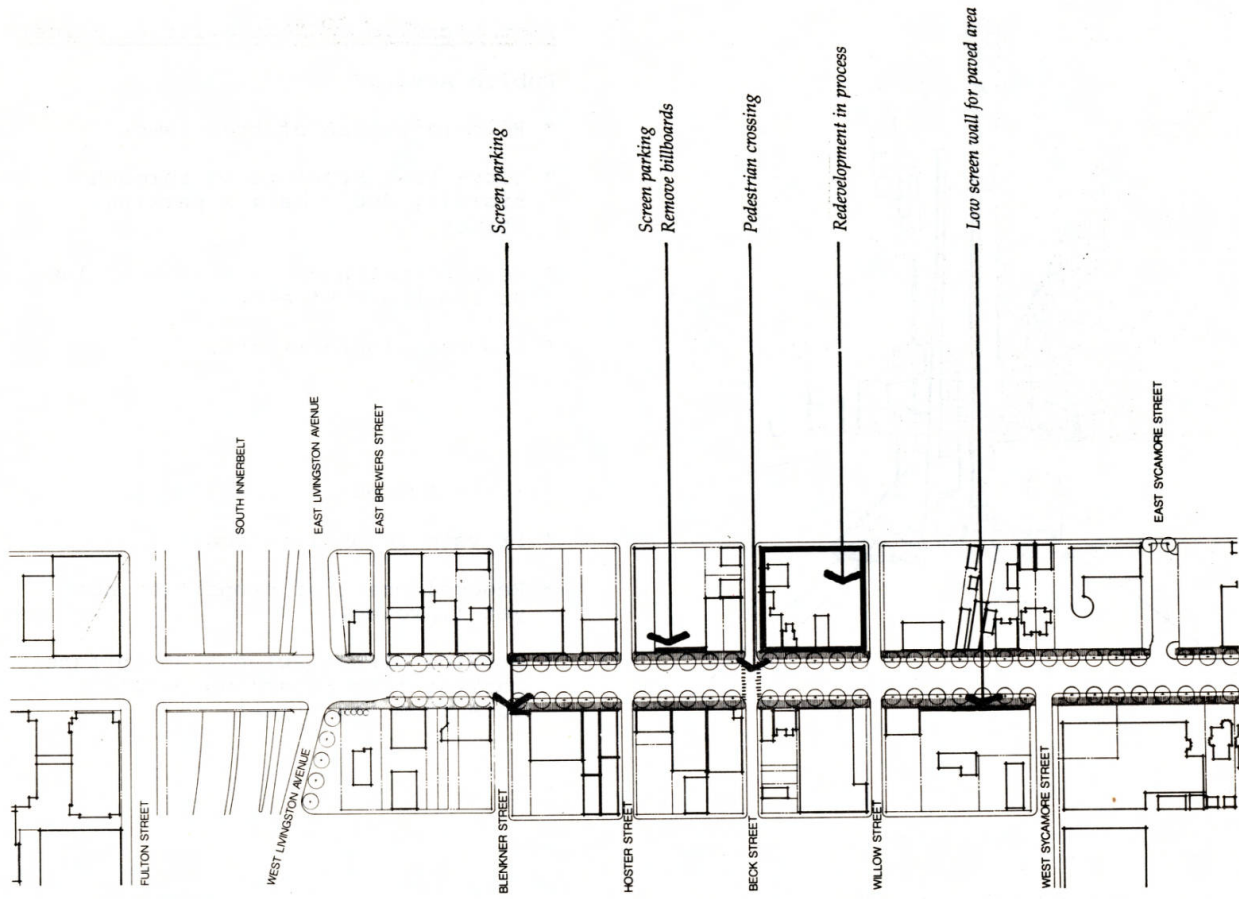
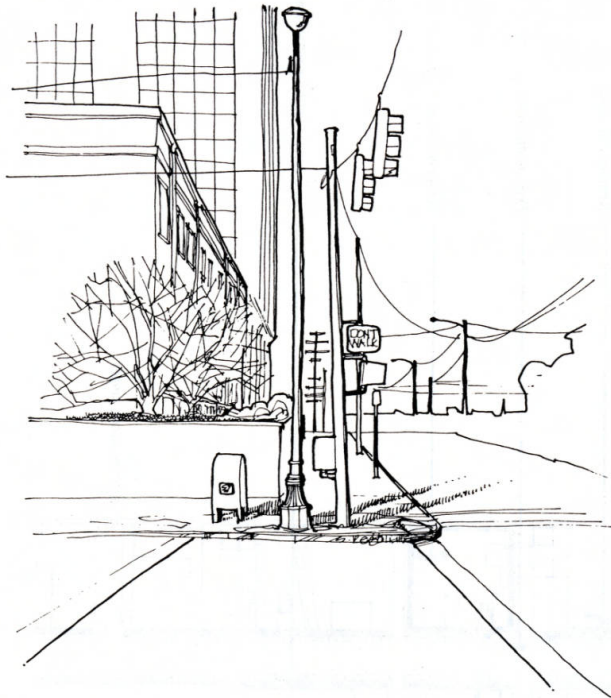


Figure 5 District 1
Livingston Ave. To W. Sycamore St.



East Sycamore to West Whittier Street:

Public Action:

- * Reintroduction of tree lawn.
- * Close East Sycamore to through traffic, and create a parking street.
- * Develop entrance to German Village at Frankfort Street.
- * Sidewalk improvements.

Private Action:

- * Private rehabilitation.
- * Redevelopment of properties not yet improved.
- * Provide visual screens at surface parking lots to screen parking lots from the street.

Figure 6 *Typical Existing Crosswalk*

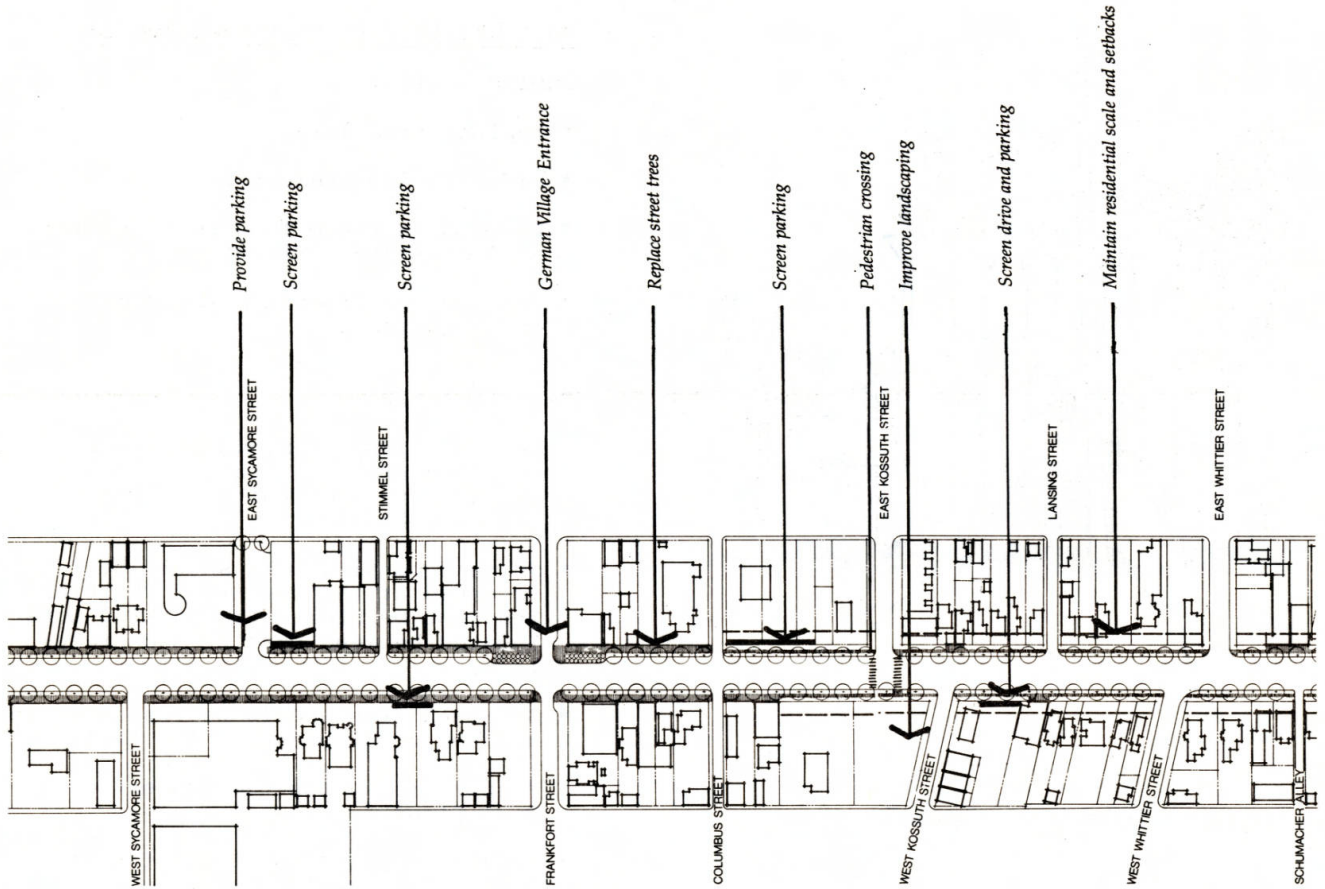


Figure 7 District 2
W. Sycamore St. to W. Whittier St.



Figure 9 Typical Existing Crosswalk

East Whittier to Greenlawn Avenue:

Public Action:

- * Replant tree lawn.
- * Sidewalk improvements.
- * Develop entrance to German Village at East Stewart Street
- * Realign the Greenlawn/Thurman intersection.

Private Action:

- * Redevelopment on Stewart Street should conform to standards developed for this portal street to German Village.
- * Surface parking lots should be screened at the corner of Whittier and West Deshler; also parking lots located between Deshler and Greenlawn.
- * Set-back line should be respected in all new development in the area.
- * Restore/redevelop the properties at East Stewart Street.
- * Redevelop parcel of land at East Deshler and High Street.

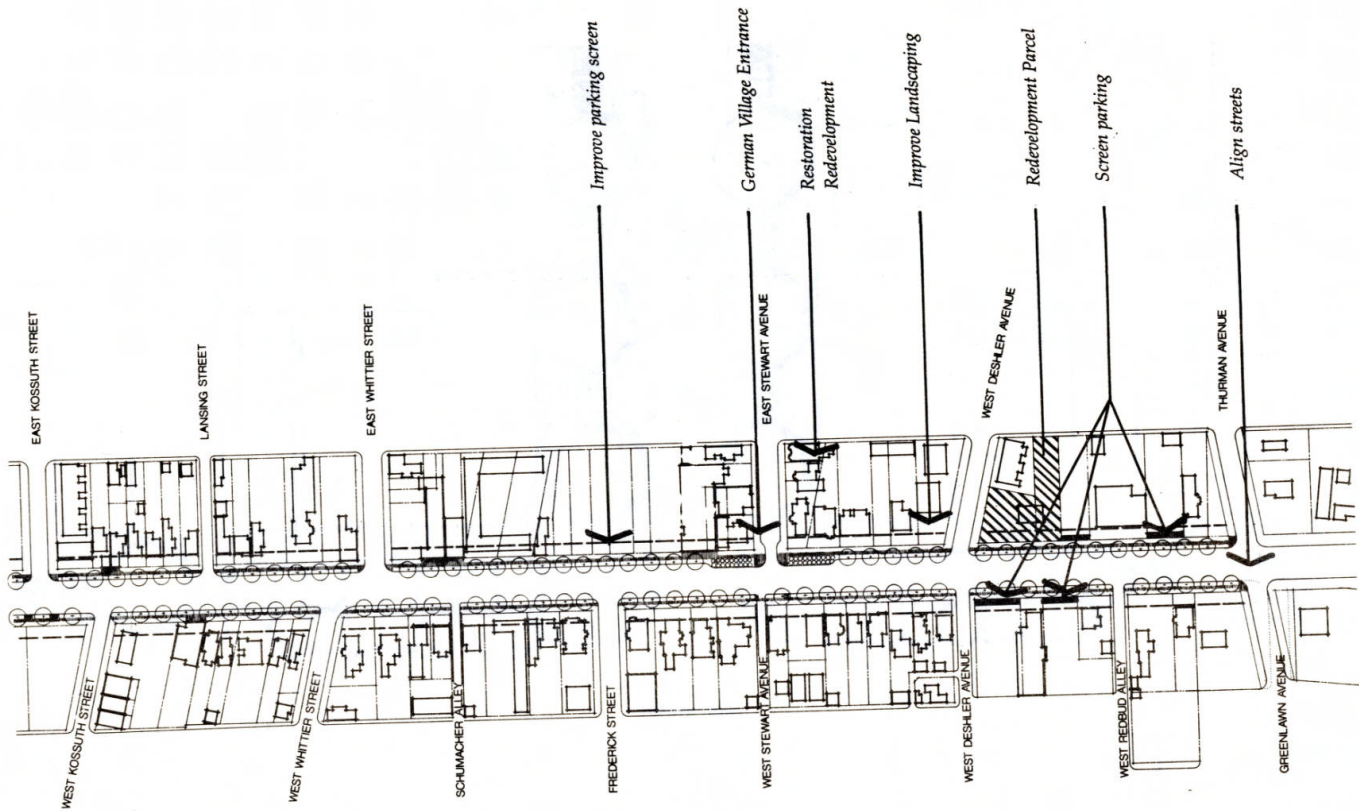


Figure 10 District 3
E. Whittier St. to Greenlawn Ave.

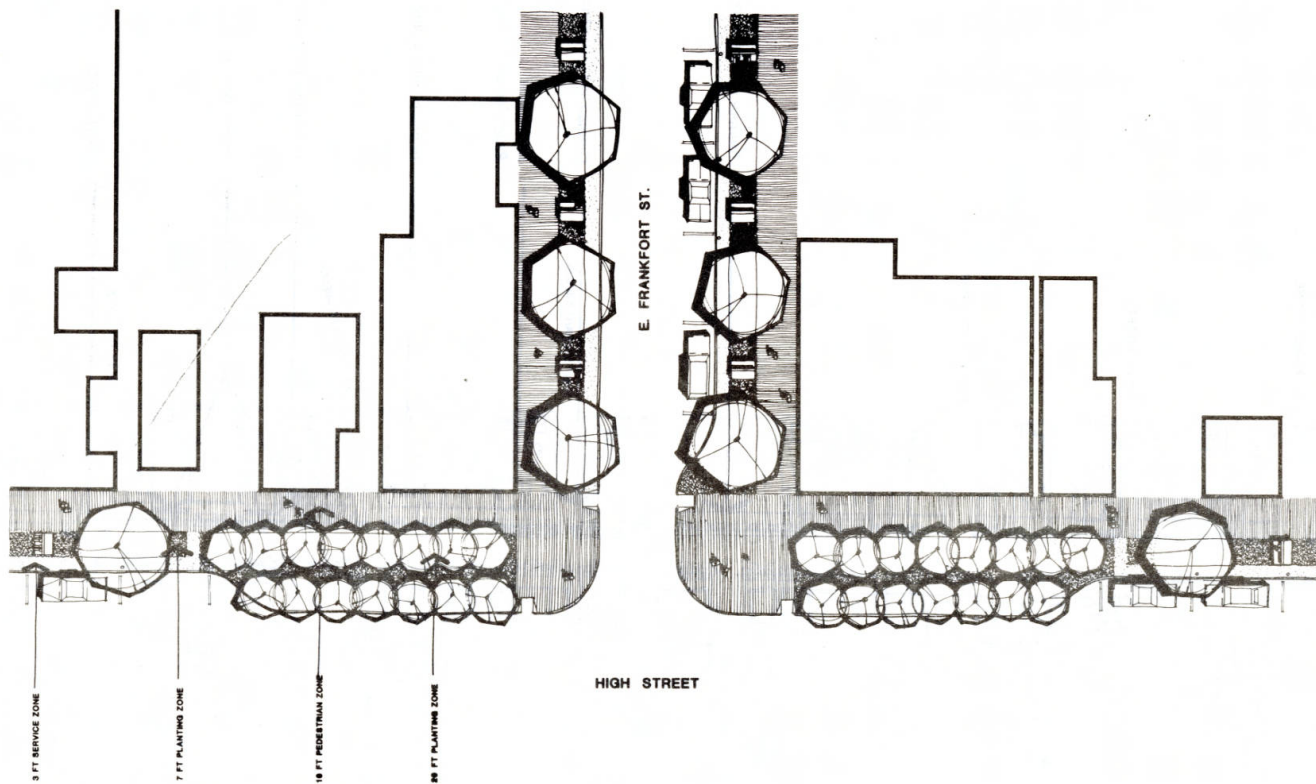


Figure 11 *Plan of Proposed Entrance to German Village*

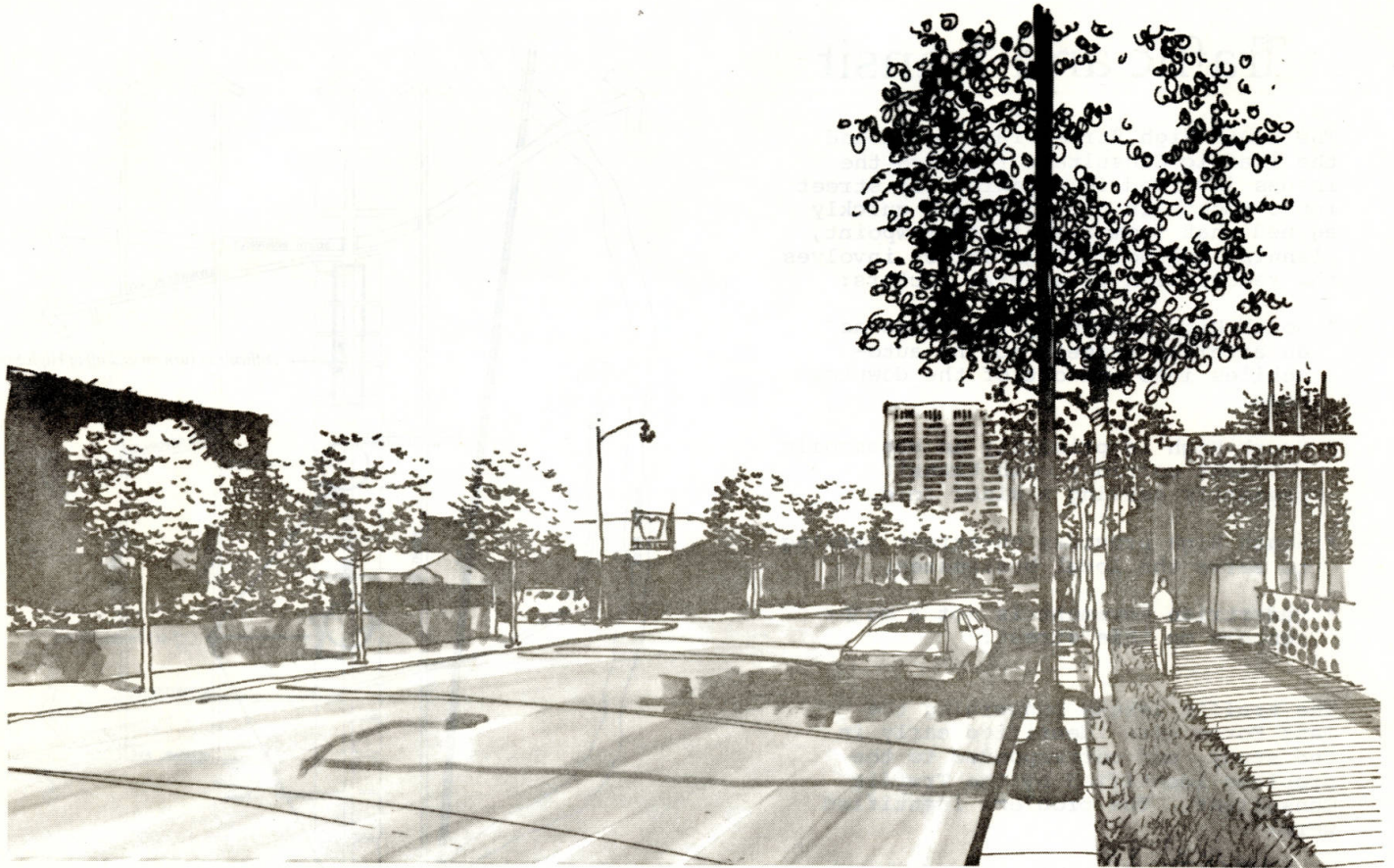


Figure 12 *View of High Street Showing
Proposed Improvements*

Traffic and Transit

The South High Street Task Force and the Traffic Consultant reviewed the issues involved in planning for street improvements in the area, and quickly agreed that from a traffic viewpoint, planning for South High Street involves the recognition of several factors:

- * South High presently functions as an arterial street, moving automobiles into and out of the downtown area.
- * South High is primarily an automobile movement corridor with transit presently serving a minor role.
- * South High provides on-street parking to serve the local businesses.
- * Pedestrian movement is not a major concern on this street.

The traffic issues concerning the Task Force were identified early in the study, and corresponded to the problems identified by the Traffic Consultant. The problems identified were:

- * Difficulty of turning movements on and off High Street.
- * Maintaining turn-over of curbside parking.

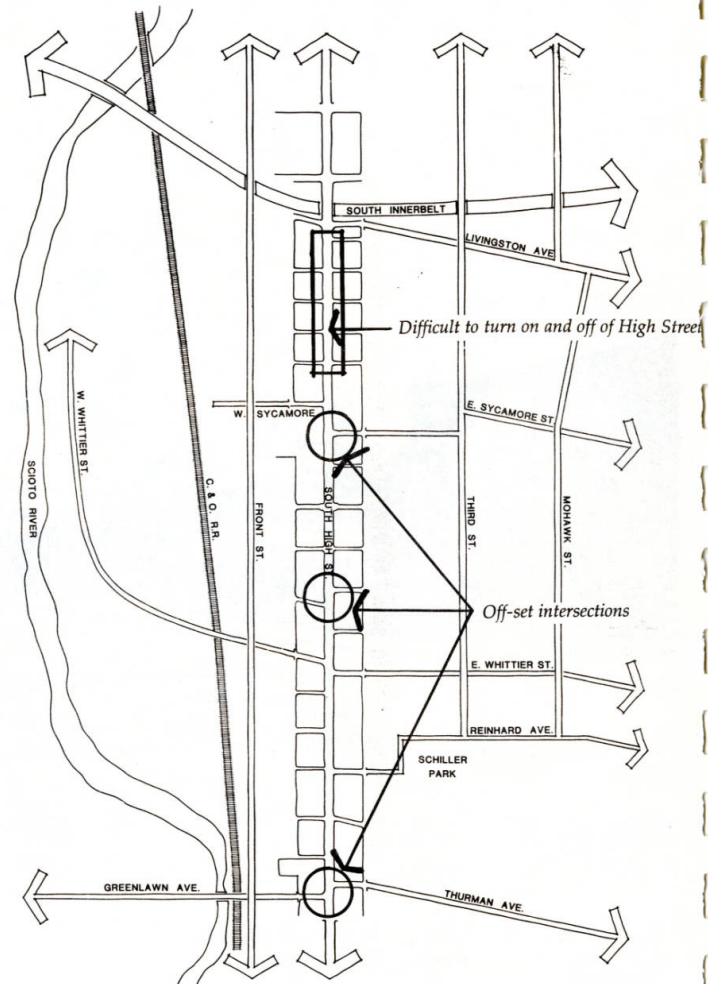


Figure 13 Existing Traffic Problems

- * Maintaining use of curbside space for parking.
- * Truck service to business establishments.
- * Identification of access points to German Village.
- * Off-set intersections create traffic movement problems.

The issue of turning movements onto and off High Street is presently concentrated in the northern section of the district, from Sycamore Street north to Blenker Street. These streets are narrow and parking is allowed on one side of the street almost to the intersection of High Street. Parking spaces on these streets are almost always full which effectively makes the streets one traffic lane streets. If a vehicle is attempting to turn into the side streets from High, and another is exiting, the vehicle on High Street must wait until the exiting vehicle turns onto High Street.

There are several solutions to this problem:

- * Limit parking on these side streets to a point no closer than 50 ft. to the intersection of High Street. This action would allow the car exiting from the side street, to move right, while waiting to enter High Street. This would allow another car to enter the street at the same time.

- * Eliminate parking on the side streets.
- * Make streets one-way and allow parking on one side only. This would allow parking on the streets, but alleviate congestion at the intersections.

The issue of curbside parking, both its availability and its turnover rate, is like the turning movement problem presently confined to the northern section of the district. The available curbside parking in this area is intensively used by persons working outside the district for all-day parking. The businessmen and residents of German Village are disturbed by this trend. It is anticipated that the need for parking spaces will increase.

The recent location of the Capital University Law School in the Grange Building will bring many new vehicles to the area. The Grange Company has attempted to respond to this issue with its announced construction of a 600-car parking facility on Front Street.

To relieve the problem of long-term parking, a number of solutions might be implemented:

- * Limit parking on side streets to residents of German Village only, through special legislation creating a special parking district.

- * Extend the installation of parking meters south on High Street to force turn-over of available parking spaces.
- * Establish a time-restriction on High Street and side-street parking, and combine this action with a strict enforcement program by the Columbus Police Department.

The off-set intersections create problems in east-west movements through the district. The realignment and closing of a number of the streets was reviewed. It was determined that the realignment or closing of a number of streets would disrupt the traffic flow through the district or seriously damage the environmental quality of the district by removing existing buildings; therefore, realignment recommendations were limited to the Greenlawn-Thurman Avenue intersection. Street closings are recommended at East Sycamore Street only. Closing Sycamore Street would de-emphasize its use as a through-street and entrance to German Village.

To identify access points to German Village, two streets were selected (Frankfort and East Stewart Street) to receive special treatment to signify that they are portal entrances to the Village.

Traffic Recommendations:

- * To improve traffic movements onto and off High Street, change Blenkner, Hoster, Beck and Willow Streets to one-way streets in alternating directions.

- * Limit parking on High Street and adjacent side streets by time-limit ordinances combined with strict enforcement of time limits by the police department.
- * Realign the Greenlawn-Thurman Avenue intersection to create a smooth continuous flow of traffic through this intersection.
- * Transform East Sycamore Street into a parking street.
- * Identify Frankfort and East Stewart Streets as access streets to German Village.

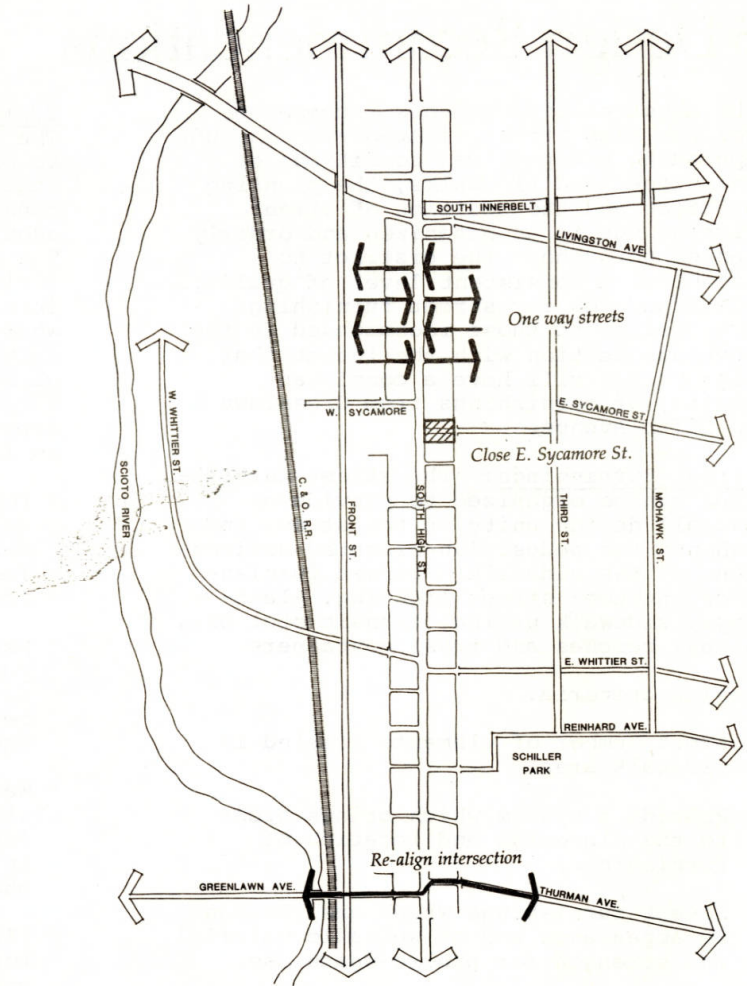


Figure 14 Traffic Recommendations

5. Design Recommendations

The physical improvements recommended for the High Street right-of-way include upgrading the physical conditions of the street and sidewalks, the planting of trees and the addition of street furnishings in an organized and orderly manner throughout the district to establish a consistent level of quality. The standards for street furnishings are similar to those recommended in the Downtown Section with the intent that High Street will have a consistent quality of furnishings from Greenlawn to Fifth Avenue.

Street Furnishings: The street furnishings can be organized to provide an overall design unity to the street and enhance the pedestrian-oriented environment of the sidewalk. Street furnishings include: street lighting, plantings, sidewalk paving, transit stations, signs, benches and trash containers.

Design criteria:

- * Limit number of elements located in sidewalk area.
- * Provide a system which brings order to the placement and location of furnishings.
- * Select furnishings which are pleasant in appearance and of suitable material and strength for public-space use.

Typical street-furnishings locations:

The sidewalk within the block should be organized so that all street furnishings are organized in a logical and consistent manner. Street corners should be reserved for pedestrians. The 20-foot wide sidewalk should be divided to provide typical areas for the installation of street furnishings wherever possible, and all street-furnishing elements should be located within the assigned area.

Specific elements should be installed as follows:

- * Postal boxes or storage boxes: maximum of three per cluster (36" oc) maximum of two clusters per block face. Front of units shall be 30" from curb.
- * Newspaper vendors: Maximum of three per cluster (36" oc) maximum of two clusters per block face, situated near intersections; front of units should be 30" from curb.
- * Waste receptacles: Should be located singularly, approx. 120' o.c. or more frequently as required, and centered in the 36" modules; front of units should be 30" from curb.
- * Flag and banner poles: Should be located singularly approx. 120' o.c. centered between every other decorative light fixture along the street curb and aligned with each other on

the opposite side of the street (alternating with waste receptacles); the centerline of the poles on both sides of the street should be 18" from the curb.

- * Transit information graphics: centerline of post shall be 18" from curb; double posts should have the centerline of the sign face 18" from curb.
- * No-parking graphics: centerline of post should be 18" from curb.
- * Traffic-control graphics: should be 18" from curb, maintaining a minimum of 6" from the edge of any sign face and the back of the curb.
- * Telephones: are not recommended in the public right-of-way in the area; where possible, pay-phones should be installed in public areas of buildings.
- * Drinking fountains: in the public right-of-way are not recommended.
- * Bicycle racks: in the public right-of-way are not recommended; however, bike storage should be included in new or renovated facilities.
- * Fire hydrants: should be painted safety-yellow and, if relocated, should be centered 18" from curb.

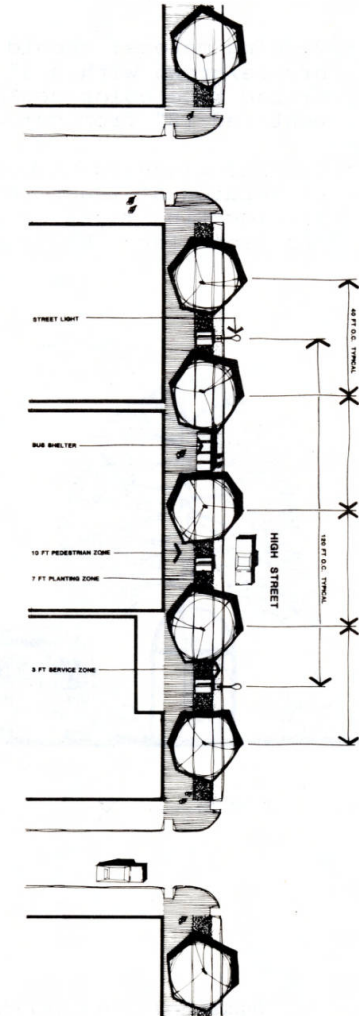


Figure 15 Typical Streetlight and Tree Spacing

- * Parking meters: should be dark bronze/brown with a 3" reflectorized tape color coding strip, centered 18" from curb.
- * Traffic signal controllers: should be located at least 30-ft from the intersecting right-of-way line and the front face 30" from curb.

- * Emergency call-boxes: should be located near the tangent point and the front face 30" from curb.

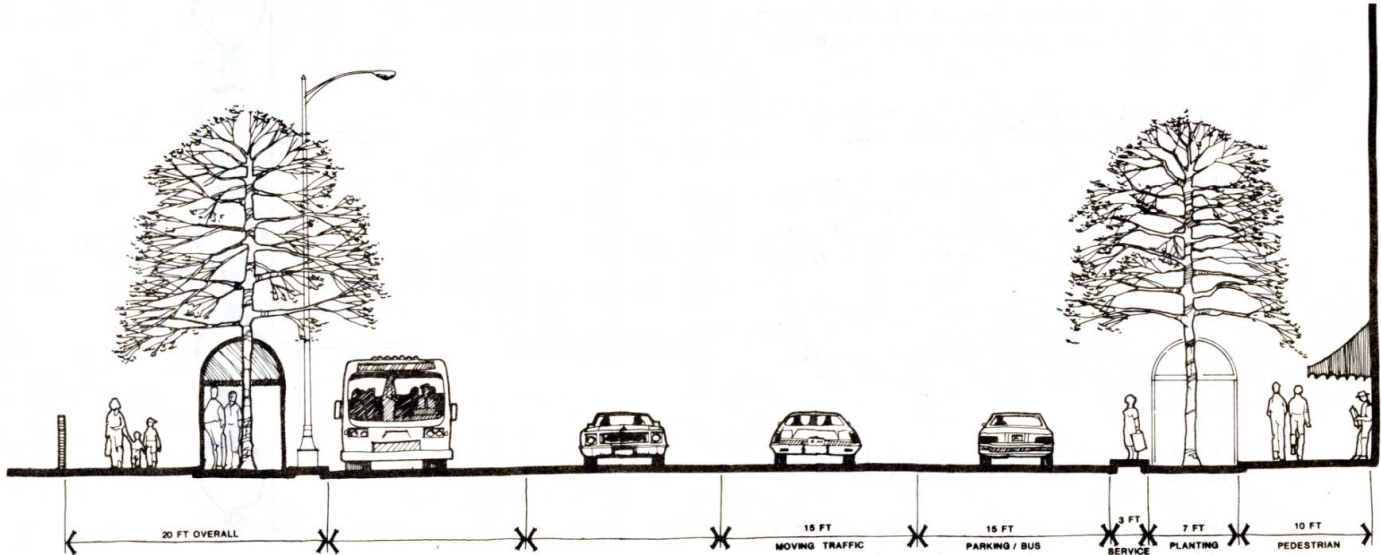


Figure 16 Typical Section Through High Street
 Indicating Zones of Use

CURB USE GRAPHICS

NEWSPAPERS

MAILBOX

TRAFFIC SIGNAL CONTROLLER

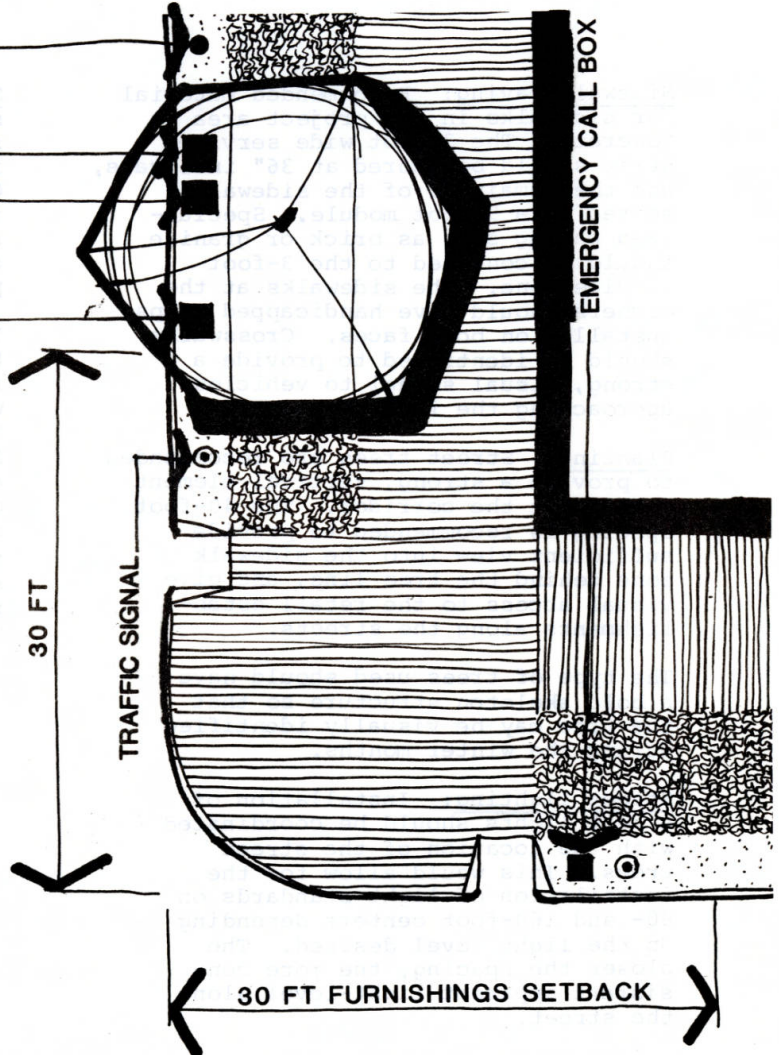


Figure 17 Typical Street Furnishings

Sidewalk paving: Recommended material for sidewalks in the project area is concrete. The 3-foot wide service strip should be scored at 36" intervals, and the remainder of the sidewalk scored in a 9-foot module. Specialized paving such as brick or granite should be confined to the 3-foot service zone. The sidewalks at the corners should have handicapped ramps installed on both faces. Crosswalks should be identified to provide a strong, visual signal to vehicles approaching the intersections.

Planting: Street trees are recommended to provide a strong, vertical element throughout the corridor. The 40-foot spacing is recommended to provide sufficient view into the sidewalk zone behind the tree line, and give visual access to the retail establishments along the streets.

The type of trees used should have a bold skeleton structure so that the tree may be visually identified during the winter months.

Street lighting: Installation of street lights should be coordinated with the location of the street trees. This would allow for the installation of light standards on 80- and 160-foot centers depending on the light level desired. The closer the spacing, the more consistent apparent light-level along the street.

Street graphics: Public-information signs typically located along the street should conform to standards published in the "Ohio Manual on Uniform Traffic Control Devices (OMUTCD)". Graphics for off-street parking should be standardized to Department of Development standard identification of off-street parking facilities.

The standards for street furnishings for High Street were developed by the Department of Development. These standards were introduced in the Capitol Square Study and are recommended for the High Street Study. The intent is to have a consistent quality of furnishings throughout the study area. Figures 18 to 28 show the standards of furnishings recommended for the Capitol Square study area and are included here as recommendations for the South High Street area as well.

Curb-Use Graphics

"No parking" signs and other small graphics regulating curb space are to be constructed from 2" square hollow steel tubing installed at a 45° diagonal to the street.

The sign face is to be flag mounted, projecting away from the street. Both the support and the back of the sign face are to be dark bronze/brown. Fasteners should be selected which are durable, tamper resistant, and inconspicuous.

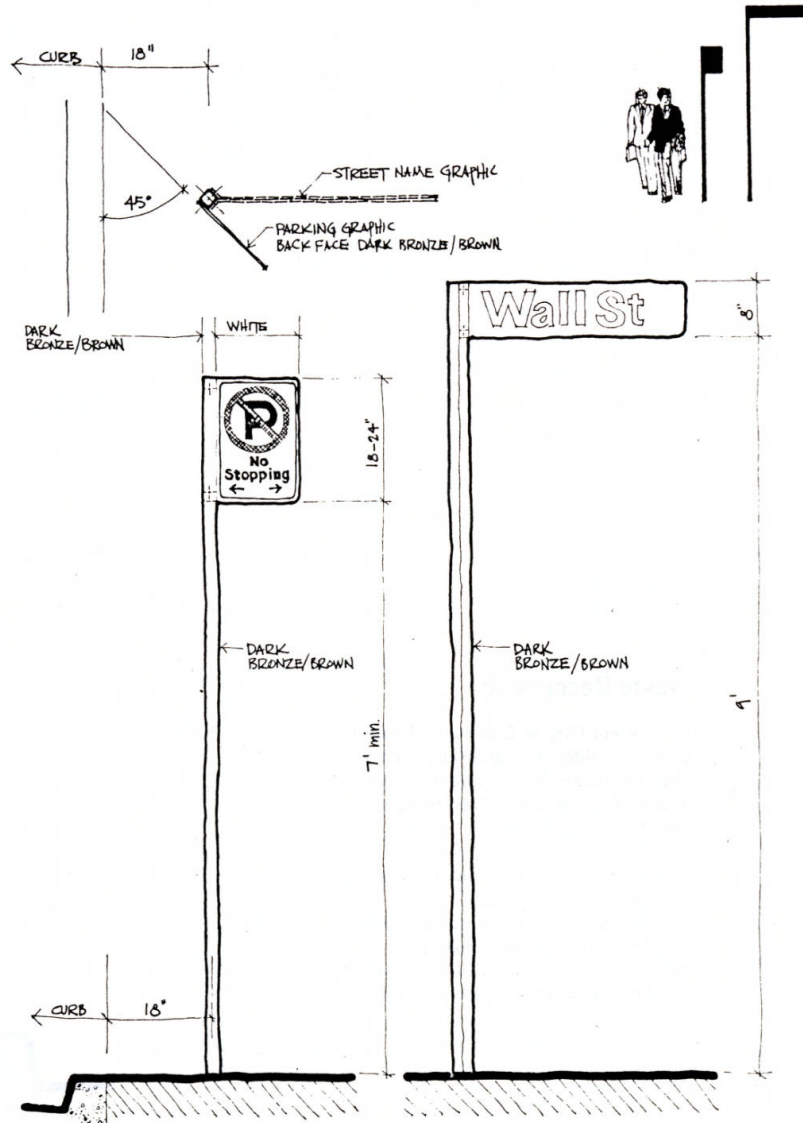


Figure 18



Waste Receptacle

The present City of Columbus standard unit is considered appropriate for the Capitol Square Area, however, more attention needs to be given to certain details:

1. Sandblasted finish, rather than exposed aggregate.
2. Rounding corners to control spalling.
3. Thinwall cast aluminum lid.
4. Increase concrete wall thickness.
5. Utilize leveling/anchoring devices.

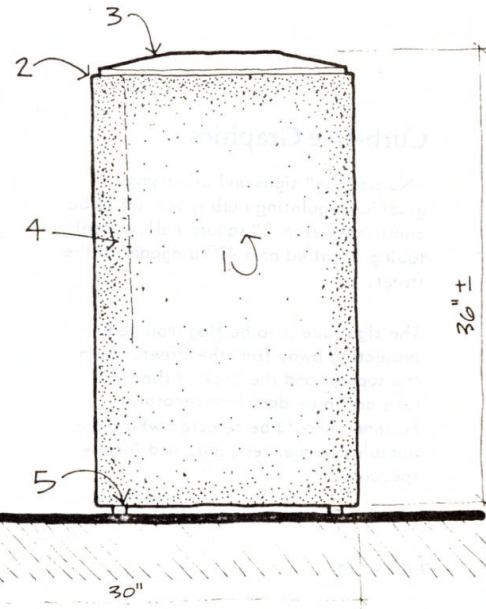
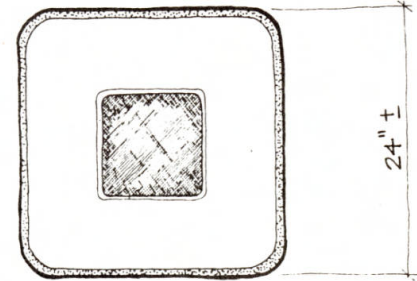


Figure 19

Graphics for Off-Street Parking

Illustrated below is the standardized graphic for identifying public-use parking facilities in Columbus. The Statehouse Parking Garage and all other facilities facing Capitol Square should give a high priority to adopting this standard. Details are available in a document published by the Department of Development titled "Identification of Off-Street Parking Facilities".

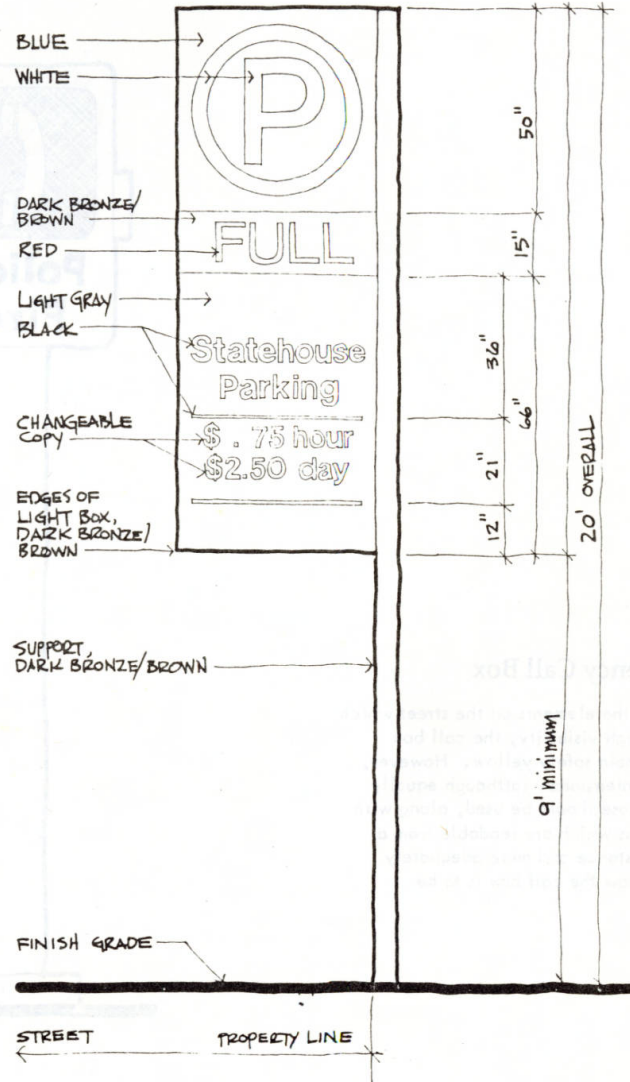


Figure 20



Emergency Call Box

As one of the elements on the street which requires high visibility, the call box should remain safety yellow. However, a more contemporary (although equally durable) case should be used, along with pictographs which are readable from a greater distance and more adequately describe how the call box is to be operated.

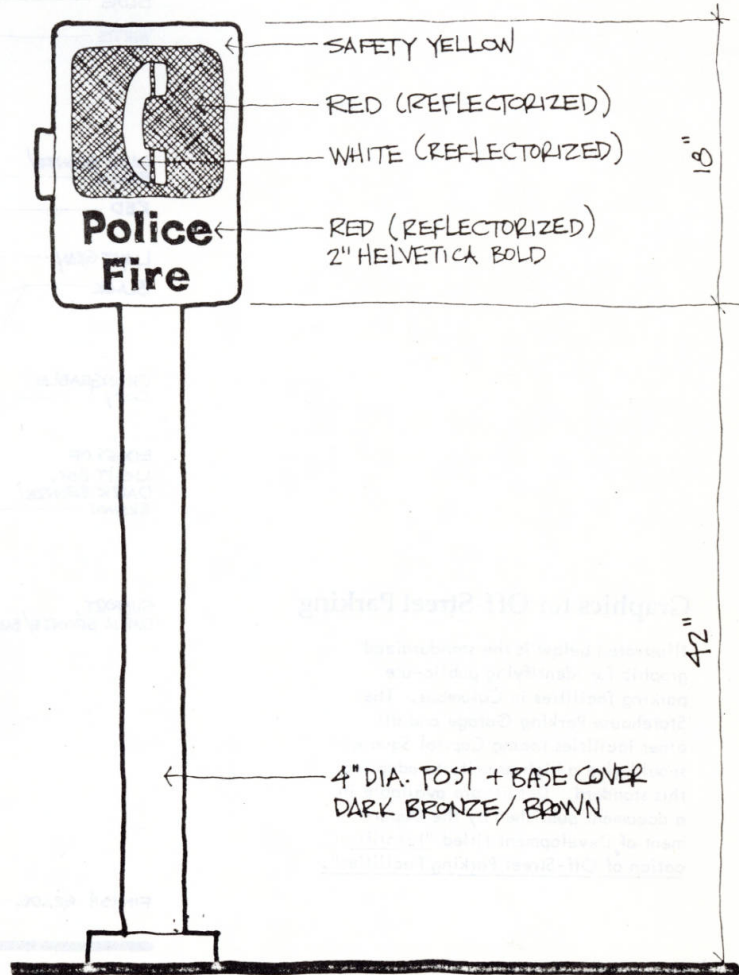


Figure 21



Traffic Control Graphics

Traffic control graphics must follow current standards as spelled out in the Ohio Manual on Uniform Traffic Control Devices (OMUTCD). However, square tubular supports, special fasteners, and dark bronze/brown finish on support and back of sign faces should be used on all such graphics in the Capitol Square area.

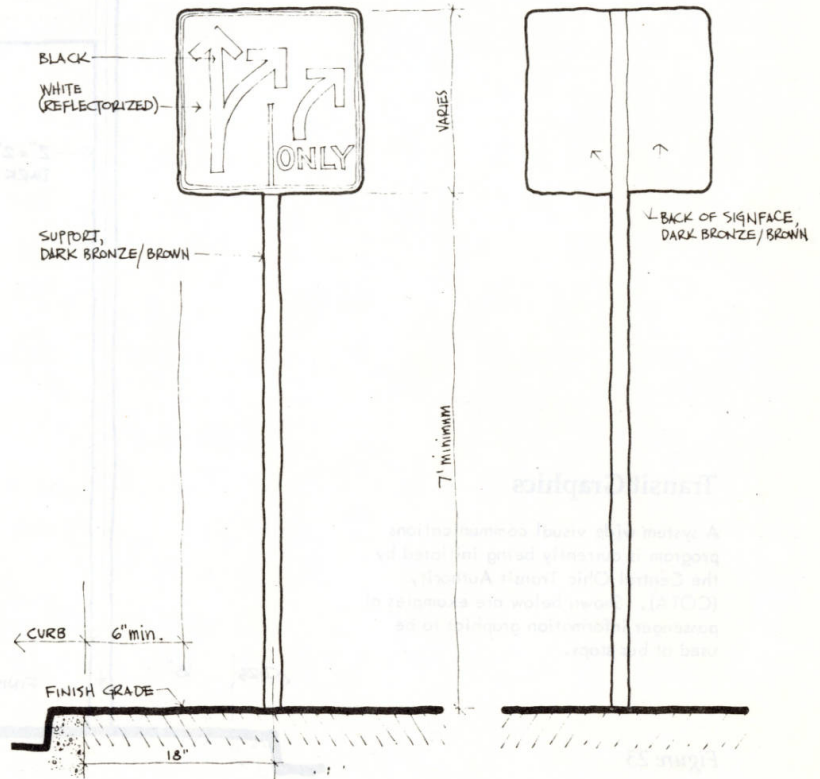


Figure 22

Transit Graphics

A system wide visual communications program is currently being initiated by the Central Ohio Transit Authority (COTA). Shown below are examples of passenger information graphics to be used at bus stops.

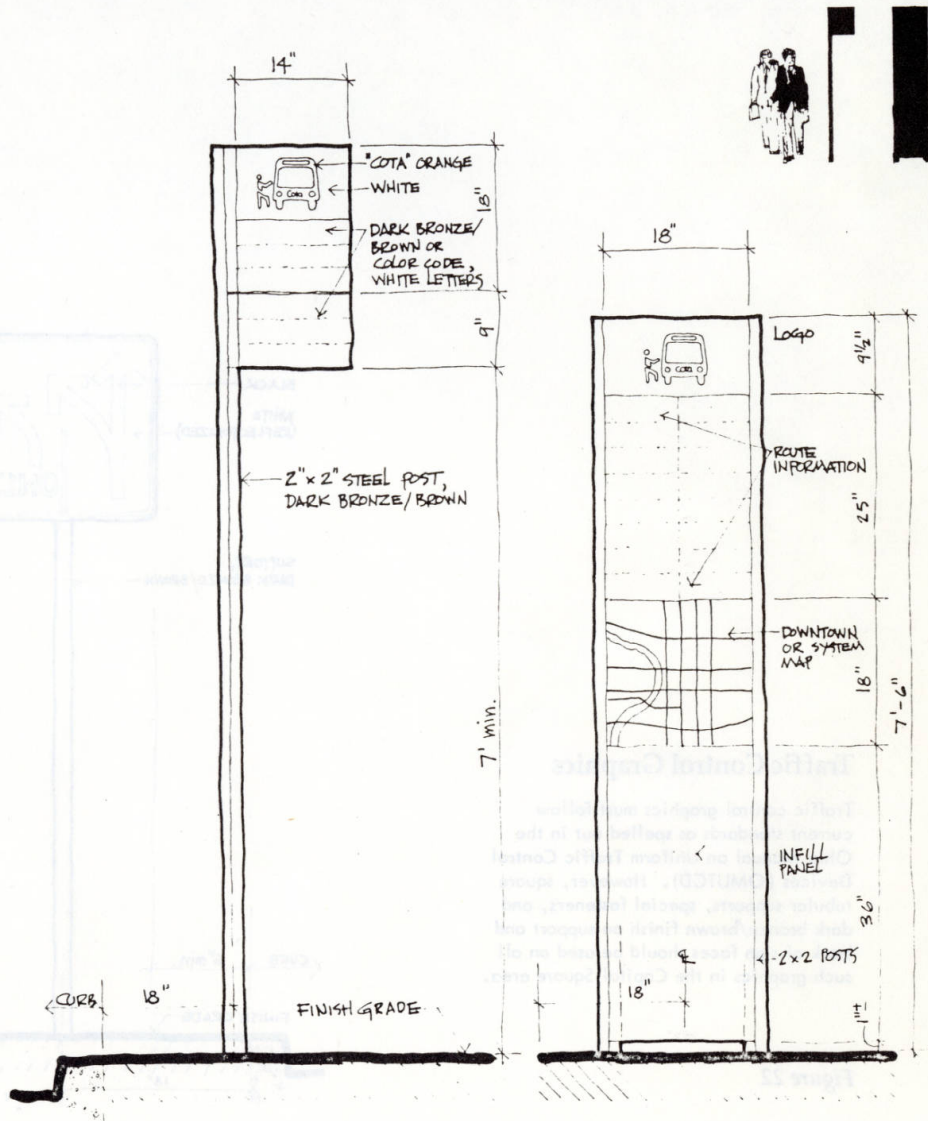


Figure 23



Signal Control Enclosure

Signal control enclosures should be as small as practicable. The illustration shows a design goal for this element, and does not reflect any single product which is currently available. These units should be installed 16" from the curb and 28' minimum from the intersecting property line.

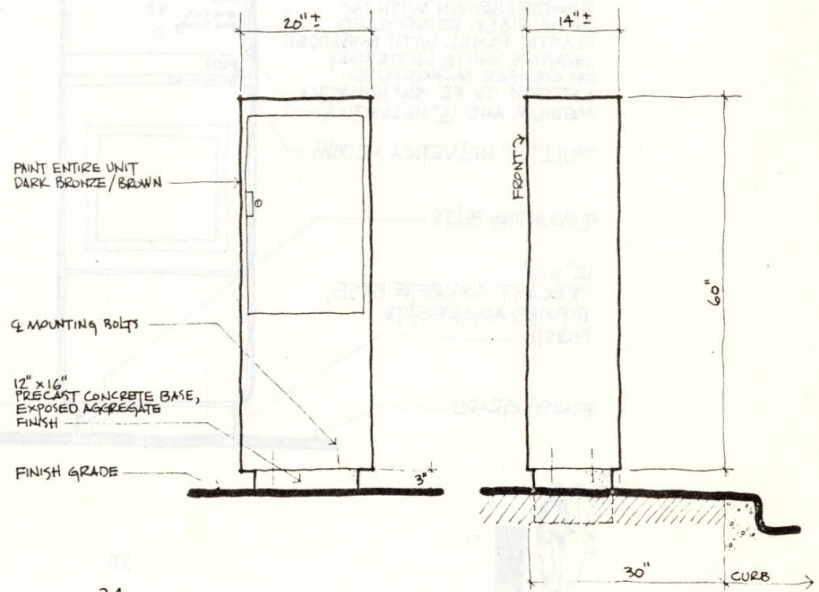


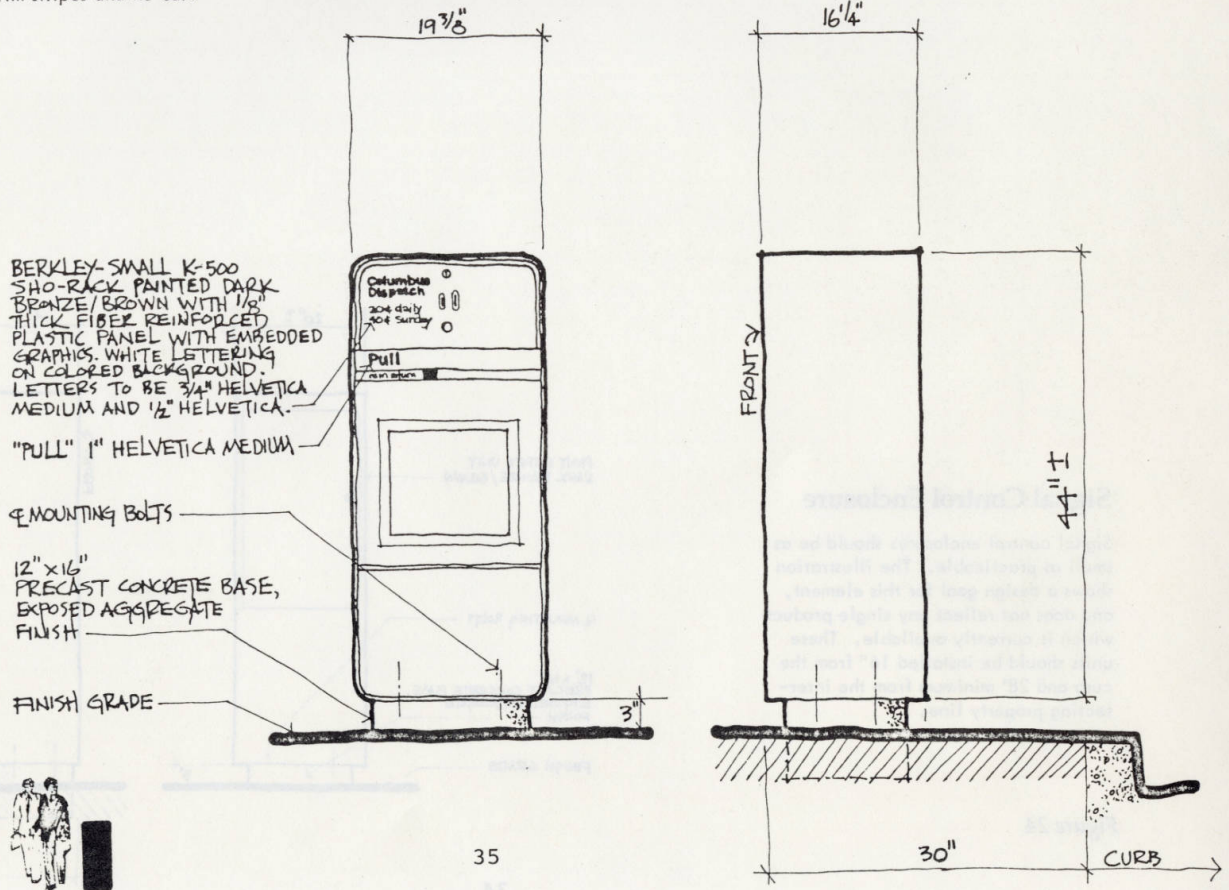
Figure 24

Newspaper Vendor

With minor modifications shown below, the newspaper vendor selected is the Berkley-Small K-500 Sho-Rack. All units are to be painted dark bronze/brown with no trim stripes and no card-

holder. Graphics are to be as shown, with lettering on a color background selected for each newspaper. Installation shall be as shown below in locations approved by the City of Columbus (see Standardized Locations, page 50).

Figure 25



Flag & Banner Pole:

The flag and banner pole is intended to be a removable structure which can accommodate graphic elements of various sizes (maximum about 3' x 8'.) These structures, with the appropriate graphic, can be installed in special sockets in the sidewalks for as long as required, then stored when not needed.

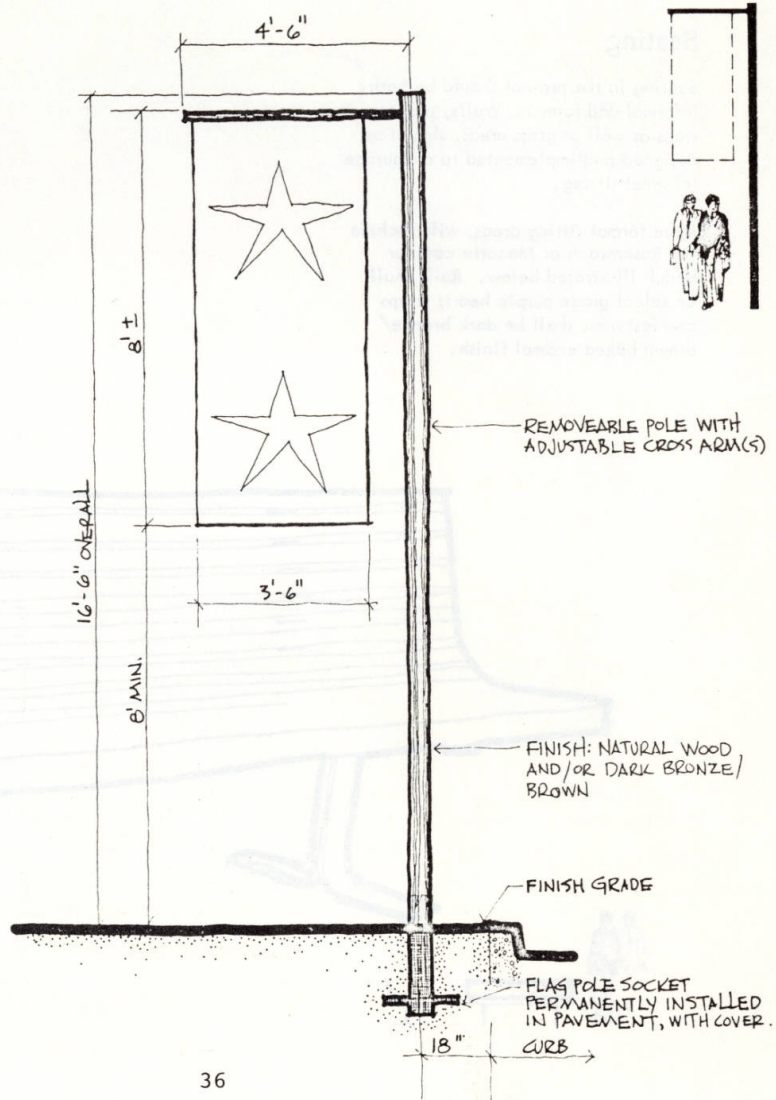


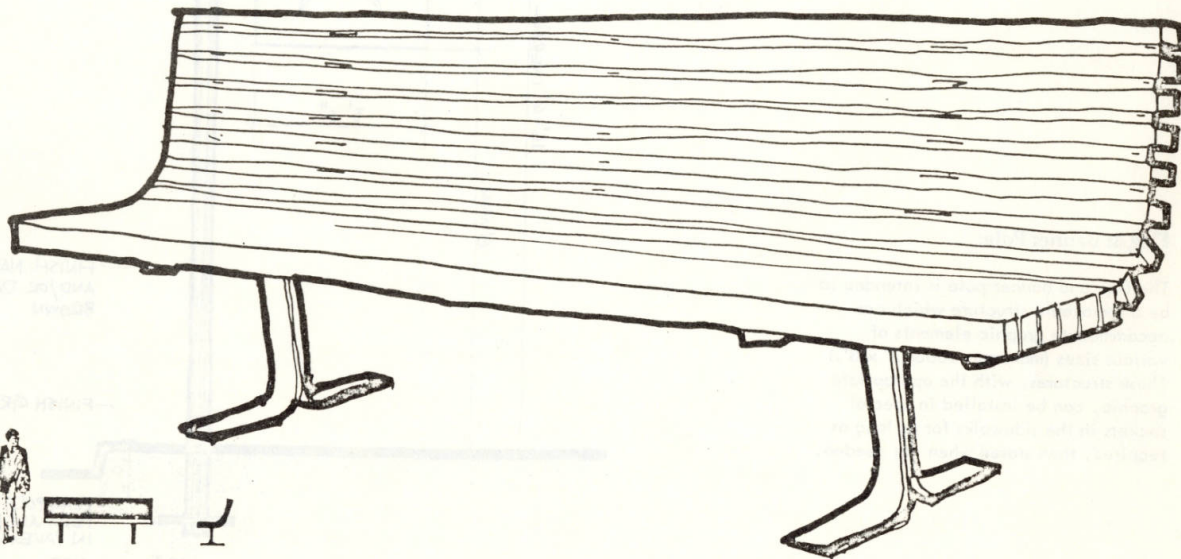
Figure 26

Seating

Seating in the project should be both informal and formal. Walls, planters, steps as well as grass areas, should be designed and implemented to encourage informal sitting.

More formal sitting areas, will include the Rosenwach or Macotta contour bench illustrated below. Rails shall be select grade purple heart; straps and fasteners shall be dark bronze/ brown baked enamel finish.

Figure 27



- * Bring unity to some of the design elements on private property along the street.

The following recommendations should be observed:

- * New building projects should respect the existing building set-back lines.
- * From West Sycamore Street, south to Greenlawn Avenue, the fore-courts or frontyards should be planted, and not paved for parking areas.
- * New building projects should fit-in with the existing scale of structures on the street.
- * New building projects and renovation projects should use materials which are similar to that already in use on the street at the present time. The predominate building material is brick.
- * Sidewalks along the street should have a uniform treatment of materials and finish.

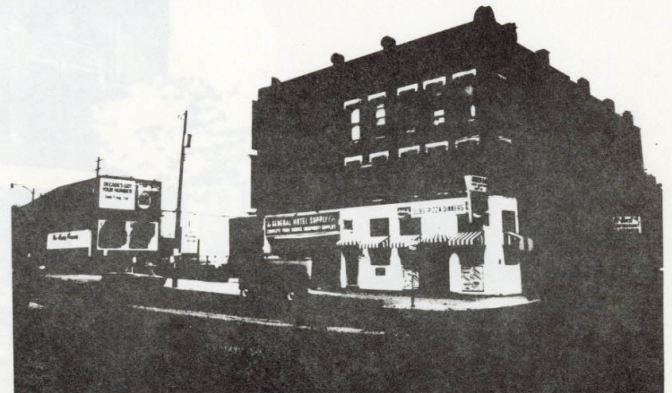


Figure 30 Existing Building Facade



Figure 31 *View of Proposed Treatment*

- * Renovation of existing buildings should reflect a design relationship between the design of first-floor shop fronts and the facade on the floors above. The architectural character of the individual buildings should be consistent from ground level to roof line. Each building should be considered as a unit, with first-floor colors and materials bearing a relationship to the floors above and respecting the character of the structure.
- * Parking lots should be screened from the street with low, brick walls, and should not rely on landscape materials alone to screen parking from view.
- * Distinctive stylistic features or examples of skilled craftsmanship which characterizes a building or structure should be treated with sensitivity.
- * The distinctive qualities or character of a building should be retained in renovation projects; the removal or alteration of historical material or distinctive architectural features should be avoided, if possible.
- * Repair or replacement of architectural features should be handled so that the new material matches the material being replaced in composition, design, color, texture and other visual qualities.

Sign Control: The city's sign-control ordinance should be rigidly adhered to in this area, and a concentrated effort should be made to improve the quality of existing signs and graphics. Businessmen and property-owners should work-together to integrate the design of signs into the architectural facade of the buildings; signs should be in harmony with the building design and not overwhelm or destroy the architectural design of the building.



Figure 32 Typical Existing Crosswalk

6 Funding and Implementation

Funding and Implementation: The implementation of the South High Street improvements will require significant financial and organizational commitments on the part of governmental agencies and the private sector.

This section of the report describes the budget for the public right-of-way improvements, potential funding sources, and the procedures necessary to move the project to actual construction.

Improvement Budgets: The plans for High Street described in Chapter 5 have been budgeted on the basis of past experience with similar projects in the city. Reviews with the City Division of Engineering have provided information on unit costs, construction standards, contingency requirements, and standard city procedures for construction contracts.

A detailed budget breakdown of the improvements, is documented in a separate appendix. The following table summarizes the budgets on the basis of functional categories to facilitate the analysis of costs for funding purposes.

Funding: Funding for the improvements to the south section of High Street is most likely to come from a combination of sources, both public and private. The options available to the city and the community include:

1) **Private funds:** The development of a funding package using only funds raised by property owners and business through the establishment of special assessments on a special taxing district.

2) **City funds:** The development of a funding package using existing financing tools available to the city through its capital improvements program, or a special program related to High Street improvements.

3) **Private and city funds:** Under this option, the city and private property owners would share in the cost of improvements. The private funds would be provided through a special assessment, and city funds through its capital improvements program.

4) **Private, city and federal funds:** This option combines private and city funds with federal funds available under federal programs which are applicable to the High Street improvements.

The present status of improving economic conditions on South High Street limits its eligibility for federal assistance to accomplish the suggested improvements to the right-of-way under existing programs. This makes Option 3 the most likely program to fund the improvements.

This option provides a method for private property owners and the city government to work together to accomplish the improvements. This option implies that private owners would be paying a share of the costs of improvements for some benefit which accrues to them while the city's contribution pays for the improvements which benefit all of the district. The city and property owners would develop a funding program and agree on a cost-sharing formula.

Implementation: The development of a program for improvements will require the cooperation and coordination of public and private groups to obtain the necessary contracts between the city and property owners in regard to the cost-sharing formula.

A checklist of actions required to carry the project forward includes:

- * Final review of recommendations by the Task Force and Study Review Committee.
- * Adoption of the plan by the appropriate local agencies and city council.
- * Establishment of a lead city agency for implementing the project and designation of a project coordinator within that agency.
- * Development of the funding program.

- * Adoption of necessary ordinances required to implement the program.
- * Preparation of technical and financial studies, if required.

One of the most important items in the checklist is the designation of a project-coordinator whose role would be to facilitate communication between local agencies, private groups, state and federal agencies, and project consultants. This role is vital to the smooth flow of information and the timely completion of tasks by various participants.

The continuation of the Task Force role is also important. As a group, the Task Force is a reflection of community concerns and can advise the city on matters related to landowners, business-ment and residents. Individually, the Task Force members can assist the city and its consultants on specific coordination issues. Finally, the Task Force can be an effective body for determining policy in situations where choices have to be made between competing interests or basic program decisions must be reassessed due to unforeseen circumstances.

HIGH STREET URBAN-DESIGN STUDY

South High Street Sector

Cost Estimate by Work Element

Catagory	Basic Cost	Contingency @ 10%	Plus Fees for Survey Inspection @ 20%	Plus Escalation to 1981 (10% x 1.5 yrs) = 15%
1. Basic Street/Sidewalk Improvements	\$956,595	\$1,052,254	\$1,262,704	\$1,452,109
2. Transit Facilities Bus Shelters	63,000	69,300	83,160	95,634
3. Furnishings/Landscape	205,520	226,072	271,286	311,978
4. German Village Entrance	518,080	569,888	683,865	786,444
Total 1980				\$2,646,165

7 Economic Development

The street improvements being proposed for the South High Street section are modest and are designed to compliment the existing street environment along much of this section; the improvements will have little impact on the economic development of the area.

The economic development of this section has already been stimulated by other forces which have brought about a dramatic turn-about of the land values and demand for commercial office space.

This section of the street was originally residential in nature with some local retail serving the adjacent neighborhoods. In the blocks immediately south of Livingston Avenue, some commercial and light industrial and warehousing uses developed. The largest commercial activities in the area were the brewery's located on Front Street.

The commercial activity of the area declined somewhat as the adjacent neighborhoods declined and the brewery's closed until the north portion of the street was anchored by those businesses which relied upon a region-wide draw rather than local business opportunities.

Several events occurred which brought about a change in the economic potential of the street:

- * The renovation and redevelopment of German Village

- * The expansion of the county complex
- * The development of the Grange Insurance Building
- * The development of the combined county-city courts complex just north of the inner-belt.

The revitalization and redevelopment of the German Village area made South High Street a more attractive location for professional office space and the residential uses on the street were slowly replaced by converting residential structures to commercial uses.

The expansion of the Franklin County complex and the development of the new Courthouse building brought about a new demand for office space in the south section of the downtown. This could not be met in existing buildings in the downtown area and South High Street area because of the under-utilization of building space, began to capture part of the office space demand through the continuing conversion and replacement of residential Courthouse Square.

The decision of Grange Mutual Insurance Company to remain in the South High Street area and develop their new headquarters building firmly established South High Street within the project area as the newest location for commercial office space in the neighborhoods immediately surrounding the downtown sector of Columbus.

It was apparent at the beginning of this study that the decline of South High Street had been arrested and that the residential uses which formerly constituted the major occupancy on the street had rapidly changed to commercial use.

The study identified two under-utilized sites in the study area which could be redeveloped; both of these sites have since been purchased and redevelopment plans are underway.

The announcement of the development of a second court building in the county complex bringing together in one location all of the city and county court functions, resulted in an additional demand for office space in the southern section of the downtown and in the study area. This demand has brought about increased pressure for office space in the study area, a demand for space that is not presently available. The result has been a rapid increase in property values over the past 18 months.

The announcement that the Capital University Law School will occupy the former Grange Building on High Street suggests that the north section of the study area and the southern section of the downtown study area will become the legal center of Columbus. It is anticipated that there will be a continued demand for office space in the study area unless sufficient space is developed in the downtown sector.

The continued increase in the number of office workers along the street, and the influx of students at the law school could create a demand for some retail services particularly convenience-goods and more food-service establishments.

Under usual economic circumstances, one would expect that this demand would result in residential units being demolished and commercial structures being developed, resulting in a change in the character of the street within the study area; however, most of the residential property has already been converted from residential to commercial space, and there are few opportunities at the present time, to acquire property for development. The size of the land parcels make it difficult, at this time, to assemble larger parcels of land for large-scale development along the street.

As stated in the beginning of this section, the economic revitalization of South High Street was underway at the beginning of this study, and has accelerated during the interim period. The street has developed a definite market orientation, and the people doing business on the street like its present character and scale. This fact probably reflects the reasons why it has developed and the commercial occupants have chosen to locate there.