

COLUMBUS CITY CENTER

RICH ST

CHEERY ST

MAIN ST

FRONT ST

MOBLE ST

FOURD ST

HIGH ST

THIRD ST

FOURTH ST

FIFTH ST



THE DOWNTOWN SOUTH PLAN

OF COLUMBUS • DEVELOPMENT DEPARTMENT • PLANNING DIVISION

OH
307.3
D7516
1994

THE DOWNTOWN SOUTH PLAN

IN MEMORIAM

Gerald A. Morrow

Central Benefits Mutual Insurance Company

Marty Lieberman

Ohio Furniture Company



City of Columbus

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Prepared for
Downtown South Association
Downtown Columbus, Inc.
City of Columbus

January 1994

Adopted by Columbus City Council, April 20, 1992

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Columbus Neighborhood Design Assistance Center

Susan Fisher

Special thanks are given to:

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The Development Department, City of Columbus, for the color printing and bidding, and to

Franklin University, for providing a forum for public meetings.

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Gay Dixon, Vice-President and Treasurer
Jeanne Schull, Secretary
Gerald A. Morrow, Past President
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INTRODUCTION

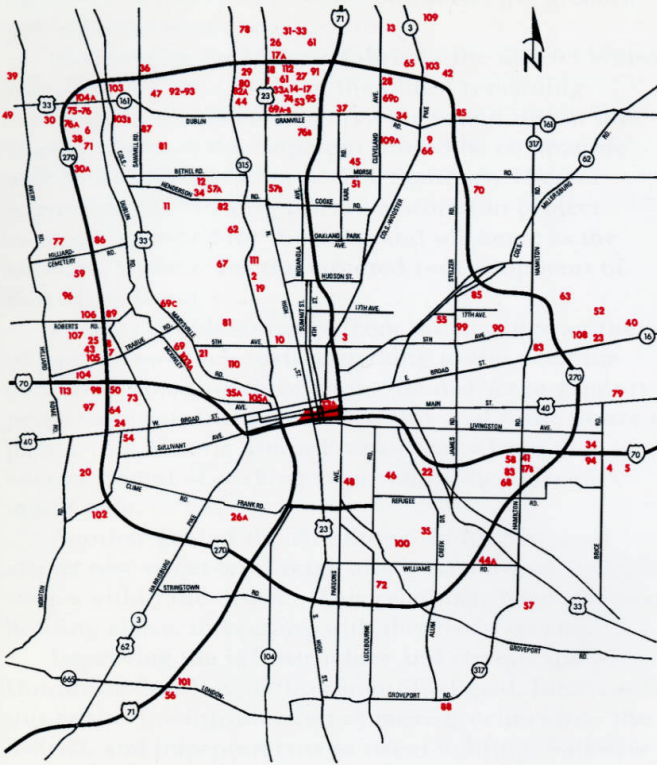
PURPOSE

The Downtown South Plan is part of an ongoing effort by Downtown Columbus, Inc. to produce specific land use plans for each downtown district. This plan represents the fulfillment of an effort to improve the character of the Downtown South district begun many years ago by Main Street merchants and continued by Downtown South, Inc. It documents the existing conditions found in Downtown South and serves as a guide for future development within the district. Additionally, this plan presents a model for future zoning decisions. Through the use of this plan, Downtown South will develop as a cohesive and vibrant district for living, shopping and working in downtown Columbus.

GOALS

Through the public planning process, several goals were identified which guided the development of this plan. First is the goal of **maintaining and enhancing the commercial character of the area**. Traditionally a business district, Downtown South contains the major Franklin County government and court complex as well as a diverse group of private sector employers. Both South High Street and East Main Street are traditional retail streets. Preserving their character and developing their retail potential are essential to implementing this goal.

Also important is the goal to **encourage residential development** within the district, and to attract the



amenities needed to support this housing. Of all the downtown districts, Downtown South has the greatest potential for residential growth.

Maintaining the historic fabric of the district is also a goal of this plan. Many of the oldest remaining buildings in the downtown area are found within this district, and new development should be compatible with these older structures. The Southern Theater represents the most significant restoration project currently planned for the area, and will serve as the cultural “anchor” for the targeted redevelopment of East Main Street.

Parking problems range from the negative aesthetic impact of too much surface parking to the need for convenient parking throughout the district to support permitted uses. Included within this goal is the desire to protect the historic Town/Franklin area from the encroachment of parking required by the adjacent urban core.

Another goal of the Downtown South Plan is to **attract new street-level retail** along established shopping streets within the district. This retail may have offices or housing above, in keeping with the urban setting.

Improving the infrastructure and streetscape of Downtown South is another identified goal. Inherent in this goal is the introduction of more greenery into the district, and improvements to street lighting, walkways and street furnishings. It is intended that Downtown South be made into a visually cohesive area, through consistent graphics and streetscape and compatibility of design.

Related to the above is the goal of enhancing the

connections to adjoining districts and neighborhoods, such as German Village, the Brewery District, Franklinton and the riverfront, Old Town East and the Capitol Square District. The plan intends to create distinct visual entrances into the area known as Downtown South.

Listed last but not of least importance, is the goal of **making Downtown South safer** for its residents, workers and visitors. New development and improved infrastructure will encourage the increased activity that will enhance public security.

GENERAL DESCRIPTION AND CHARACTER

Downtown South was traditionally a commercial and retail area with the same intense mixture of uses, prior to the developments of the last two decades, that characterized the rest of downtown. South High Street was once an unbroken commercial and street-level retail street, extending all through downtown, past the County Courthouse building into the German Village area. The Central Market and the retail enterprises which surround it once made the area the meat, produce, and dry goods shopping district for the lower

EXISTING CONDITIONS

BOUNDARIES

Downtown South encompasses approximately the southern third of Downtown Columbus. The district is bounded on the north by Town Street, on the south by Interstate 70, on the east by Interstate 71, and on the west by the Scioto River. The portion of Downtown South east of Sixth Street overlaps the Discovery District.



ACCESSIBILITY AND CIRCULATION

Access to Downtown South is easily gained from Interstate highways and from arterial streets. Freeway access connects the district with all parts of the Columbus metropolitan area. East-west street access is provided by the Rich Street and Main Street one-way pair, and north-south access is provided by the Third and Fourth Street one-way pair and by High Street, the city's major north-south artery.

Bridges are located across the Scioto River at both Main and Town Streets. Historically, Town Street and Main Street made a one-way pair. Construction of the City Center forced the closing of a portion of Town Street, and Rich Street became the other half of this pair. Rich Street, however, has no bridge, and westbound traffic must make its way back to the Town Street Bridge to continue west. The Town Street Bridge is slated for replacement during the 1990's.

GENERAL DESCRIPTION AND CHARACTER

Downtown South was traditionally a commercial and retail area with the same intense mixture of uses, prior to the developments of the last two decades, that characterized the rest of downtown. South High Street was once an unbroken commercial and street-level retail street, extending all through downtown, past the County Courthouse building into the German Village area. The Central Market and the retail enterprises which surround it once made the area the meat, produce, and dry goods shopping district for most of

Columbus. The commercial dominance of the district began to change during the post-World War II period as both housing and retail opportunities moved toward the suburbs.

In the 1960's and 70's, two major redevelopment projects occurred which would change the area's character irrevocably, but which would also serve to create a distinct "Downtown South" district, separate in character from the high-rise employment core emerging around Capitol Square. These projects were the Market/Mohawk Urban Redevelopment, which assembled the Central Market (razed in 1966) and its environs for major urban renewal projects, and the Capitol South project, which redeveloped a massive three-block area south of the State Capitol building. The Market/Mohawk project made possible the development of three and four-story office buildings, with pleasant suburban-style landscaping along Town Street, as well as the construction of high-rise senior citizen housing in the district as far east as Grant Avenue.

The Capitol South Community Urban Redevelopment Corporation transformed the traditional commercial/retail area between South High and South Third Streets over a period of nearly two decades into the modern mixed-use complex that now includes Columbus City Center (with over 1 million square feet of retail), two office buildings, the Capitol Square Hyatt Hotel, and the restored Ohio Theater. The 3,500-space parking garage and accompanying street-level retail along Third Street are attached to City Center within the Downtown South district.

Until the 1950's, manufacturing uses dominated the southeast and southwest quadrants of the district. These uses are reflected in the outdated zoning still in place. Of the 700 land parcels now contained in the Downtown South area, 347 remain zoned for manufacturing uses. However, fewer parcels than 40 continue to support uses that can be classified as manufacturing.

Near the riverfront, an area which once flanked a feeder to the Ohio Erie canal, developed during the 19th century as a warehouse and manufacturing district. A few of these early structures remain. The Old State Arsenal, now the Cultural Arts Center, is a striking visual cornerstone of the area. Many of the older buildings, however were cleared from the riverfront to make way for a Bicentennial Park. A few lighter manufacturing concerns remain, but the high-rise Waterford condominiums have set a new tone for the future of the riverfront.

Between Front and Fifth Streets lies a primarily business area. Main Street and High Street here both contain a concentration of late nineteenth century commercial structures, as well as a few larger buildings such as the now restored Great Southern Hotel. Within this zone, the seven-building county government complex occupies two city blocks. Several large parcels in this area stand ready for development, as do many small "infill" sites.

From Fifth Street to Grant Avenue is the area which was totally transformed by the urban redevelopment movement of the 1960's. This area now has a "campus-like" atmosphere, with much green space and

design consistency characterizing the master plans of the academic, health care, and other institutions which dominate the area. This area forms a transitional zone between the Downtown core and the residential neighborhoods to the east that are now experiencing revitalization. High and low-rise apartments also give this area an urban feeling not found elsewhere in downtown Columbus.

The fourth distinct area found in Downtown South is the area east of Grant Avenue, which was once primarily single-family homes and apartments. A small but very fine group of these homes remains, and has been designated as an historic district. Larger areas of this zone are in transition, however, and a considerable amount of land is now either surface parking or vacant. Main Street, in this sub-area, has developed a concentration of auto-oriented businesses, in contrast to the "storefront" character to the west.

In summary, the Downtown South area, once primarily a commercial and even manufacturing district, has become an area which retains a number of business and retail uses but which has made way for a mixture of new uses. Franklin County Government and Franklin University are now the two largest employers in the district. The health care and health insurance industries are also major contributors to the growth of the area: Grant Hospital, Central Ohio Medical Group, and Central Benefits Mutual Insurance Company are important employment centers. Redevelopment and changing uses have created opportunities for housing development, especially along the magnificent Scioto

riverfront. The area still retains much of its traditional retail character, especially along South High and East Main Streets, where opportunities exist to reintroduce retail uses to serve new development.

INSTITUTIONS AND ACTIVITY CENTERS

Institutions and activity centers anchor key areas within the district:

Franklin County Government Center: As the ninth largest employer in Franklin County, the county owns one of the major building complexes in downtown Columbus. The presence of county government and its related activities has led to a demand for office space in the vicinity.

Bicentennial Park: This park, opened in 1976, is actually a continuation of a riverfront beautification plan begun in 1908. The banks of the Scioto River, once a squalid industrial area, have been redeveloped over time for public parks and public buildings, in keeping with the "City Beautiful" movement of the early twentieth century. Today the riverfront is the primary location for downtown recreational activities.

Cultural Arts Center: Perhaps the most visually striking building downtown, this former arsenal is now operated by the Recreation and Parks Department as an arts center.

Southern Theater: Located in the landmark Southern Hotel, this theater is owned by the Columbus Association for the Performing Arts. Restoration of the Southern Theater, a project highly ranked by participants in the 1988 Downtown Strategic Planning

process, involves plans for this intimate theater to supplement the much larger Ohio Theater. The Southern Theater's highly visible location at East Main and South High Streets, and its relationship to the proposed Main Street target renovation, enhance the theater's status as a cultural "anchor" for this retail/commercial district.

Columbus City Center: Serving fourteen to fifteen million visitors annually, this is the primary activity center in the downtown. Although only partially located within Downtown South, it has served to bring a major retail presence back into the district.

Franklin University: This urban campus stretches for three blocks along Grant Avenue. Offering day and evening classes to four thousand students, this campus is a major center of activity. With the construction of a new "Campus Green", Franklin University has taken on a unified and collegiate appearance.

Grant Hospital and Central Ohio Medical Group: Together, these two institutions represent a substantial employment center for the health care industry. Located along Grant Avenue and Town Street, these institutions can be expected to contribute significantly to future development of the Downtown South district.

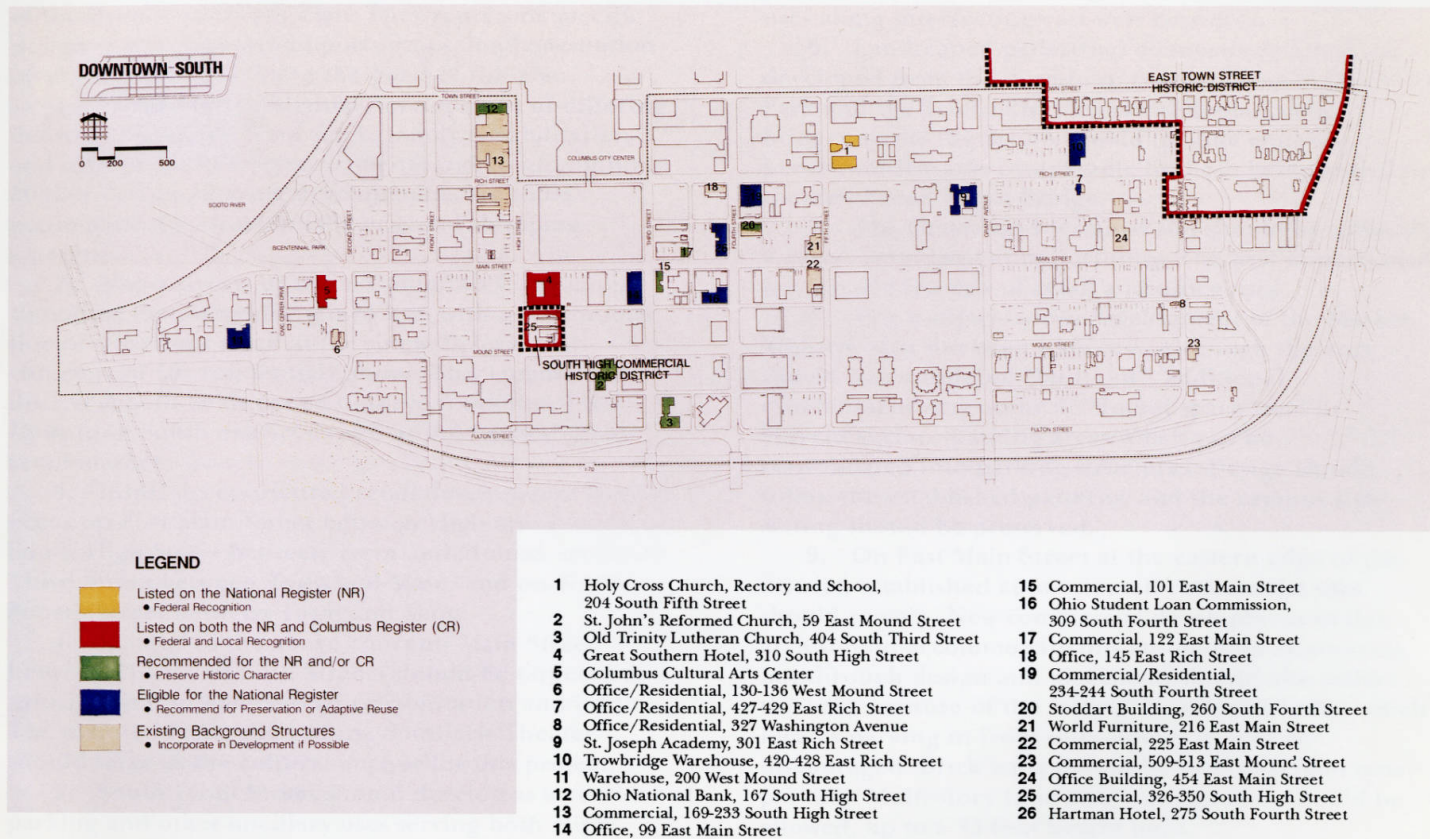
HISTORIC STRUCTURES

Part of the distinctive character of Downtown South is the presence of many older historic or architecturally significant buildings. These buildings are found throughout the district, even within the urban redevelopment area. Several of these historic structures are religious or cultural facilities with a strong visual presence. Many more are commercial structures which range from small storefront buildings to warehouses and hotels.

Because of these older buildings, Downtown South maintains a pedestrian scale and a feeling of connection to the past. Most of these older buildings are still in active use today; a few present unique opportunities for adaptive reuse. On the following page is a listing of identified historic properties.

Number	Building Name and Address
1	Church, Backway and School, 301 South Fifth Street
2	St. John's Reformation Church, 59 East Mound Street
3	Old Trinity Lutheran Church, 604 South Third Street
4	Great Southern Hotel, 311 South High Street
5	Columbus Cultural Arts Center
6	Office/Residential, 130-136 West Mound Street
7	Office/Residential, 457-479 East Rich Street
8	Office/Residential, 287 Washington Avenue
9	St. Joseph Academy, 301 East Rich Street
10	Towbridge Warehouse, 450-428 East Rich Street
11	Warehouse, 297 West Mound Street
12	Ohio National Bank, 187 South High Street
13	Commercial, 189-253 South High Street
14	Office, 99 East Main Street
15	Commercial, 101 East Main Street
16	Ohio Student Loan Commission, 299 South Fourth Street
17	Commercial, 122 East Main Street
18	Office, 145 East Rich Street
19	Commercial/Residential, 294-291 South Fourth Street
20	Seedling Building, 293 South Fourth Street
21	World Fragrances, 218 East Main Street
22	Commercial, 225 East Main Street
23	Commercial, 309-313 East Mound Street
24	Office Building, 451 East Main Street
25	Commercial, 325-329 South High Street
26	Hortman Hotel, 275 South Fourth Street

HISTORIC STRUCTURES



DESIGN AND DEVELOPMENT OPPORTUNITIES

The following recommendations constitute the core of the Downtown South Plan. They represent specific design or development opportunities, implementation of which will help achieve the goals of the plan.

1. Encourage residential development of differing densities/lifestyles, from single family rehabilitation and infill to multi-story apartments and condominiums. Higher density housing developments are to be accompanied by residentially oriented shopping amenities.

2. Major streets near the Columbus City Center should be developed for mixed use, with a continuous line of storefront retail at the sidewalk level and commercial (or residential) above. This commercial district should be designed to serve residents of the Downtown South district as well as the Central Ohio retail market.

3. Infill, street-oriented retail development should occur on East Main Street between High and Fourth; on South High Street between Town and Mound; on South Third Street between Town and Main; and on South Fourth Street between Town and Main.

4. Related to the above concept, Main Street between Third and Fifth Streets should be targeted as a priority area for commercial rehabilitation and infill. The planned restoration of the Southern Theater should serve as the cultural anchor for this project.

5. South Front Street should develop as a corridor of parking and other ancillary uses serving both High

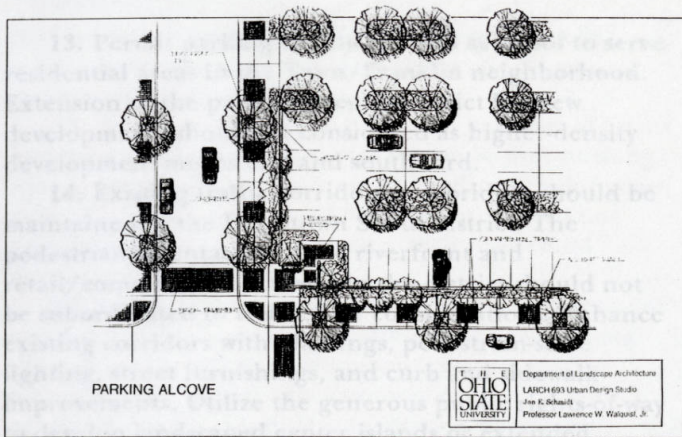
Street commercial buildings and the riverfront corridor. However, good design features, pedestrian scale, and pedestrian amenities should serve to enhance pedestrian traffic both along South Front Street and to uses along intersecting east-west corridors.

6. Landscaped pedestrian connections should be developed from the downtown core to Bicentennial Park and the Scioto River corridor. Replacement bridges across the Scioto should include enhance pedestrian features comparable to those being added to the new Broad Street Bridge.

7. The Greyhound terminal site should be utilized as a major development opportunity. The best opportunity for this site is a mix of retail and office uses.

8. The campus style redevelopment of the Market Mohawk area has been a success. Although the area should be considered “built out”, additional development may occur on “found space” and in current surface parking areas which can be consolidated into parking structures. Design should follow the established patterns, and the campus style setting should be preserved.

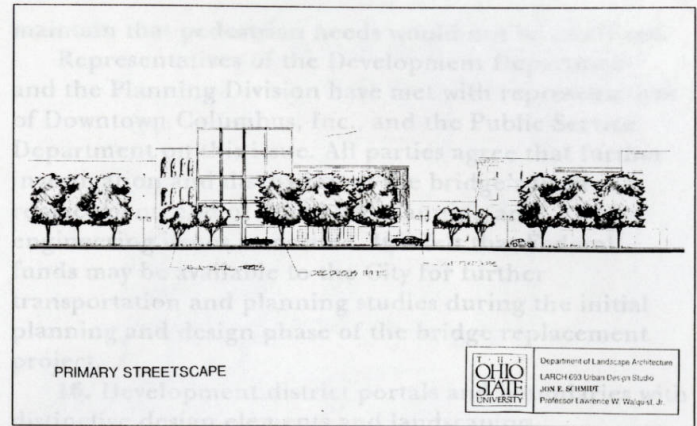
9. On East Main Street at the eastern edge of the district, established auto-oriented commercial uses should remain. New commercial development in this area should accommodate this automobile orientation, but through design and placement should also reflect the urban nature of the setting. New construction which locates parking in front of buildings should be discouraged. Brick walls should be used to screen new parking. Multi-story (mid-rise) construction should be allowed, up to a 35-foot height limit.



Screened Parking/Streetscape

10. Landscaped parking “alcoves” with architectural lighting and screenwalls should be developed by property owners to serve the retail/commercial district represented by main, Third and Fourth Streets. Brick or other decorative walls or landscaping should be used to maintain the street-front continuity. Incentives and guidelines for better landscaping of surface parking lots should be adopted.

11. The area around the intersection of Grant and Town Streets brings together the downtown commercial core, an intact historic district (Town/Franklin), and two large institutions (Grant Hospital and Franklin University). This important junction experiences conflicting demands for space and deserves sensitive treatment. Institutional parking requirements should be



Improved Streetscape/Tree Placement

met with parking structures fronting on the east side of Grant Avenue, replacing a continued reliance on larger surface lots. These structures should have pedestrian amenities such as street level commercial space and landscaped sidewalks, and should blend in scale and materials with both the campus-style setting west of Grant Avenue and the historic district to the east. Second-level walkways across Grant are acceptable within the campus context.

12. As parking structures are developed to serve Franklin University and Grant Hospital, large surface lots along Rich Street should be redeveloped as neighborhood scale, mixed-use commercial/residential structures to serve employees of nearby institutions, including those in the Discovery District.

13. Permit parking should be used as a tool to serve residential areas in the Town/Franklin neighborhood. Extension of the parking-exempt district for new developments should be considered as higher-density development moves east and southward.

14. Existing traffic corridors and bridges should be maintained in the Downtown South district. The pedestrian orientation of the riverfront and retail/commercial areas within the district should not be subordinated to traffic flow considerations. Enhance existing corridors with plantings, pedestrian-scale lighting, street furnishings, and curb and sidewalk improvements. Utilize the generous public rights-of-way to develop landscaped center islands or extended sidewalks. Where head-in parking is used, establish a curblane adjacent to the traffic lane by constructing landscaped pedestrian “peninsulas” at periodic locations, including mid-block pedestrian crossings.

15. The planning and engineering communities are both concerned over the location of the Town Street Bridge. The planners content that the bridge should be retained at its existing location to preserve the pedestrian needs of the area and to maintain the present “frame” for the Scioto Peninsula riverfront provided by the symmetry of the present Town and Broad Street Bridges.

The engineers contend the bridge should be located to align with Rich Street on the east to regain the downtown’s east-west arterial system that was lost with the construction of the City Center Mall and to provide for the increase in traffic expected through the immediate areas as a result of its redevelopment. They

maintain that pedestrian needs would not be sacrificed.

Representatives of the Development Department and the Planning Division have met with representatives of Downtown Columbus, Inc., and the Public Service Department on this issue. All parties agree that further investigation and discussion of the bridge’s future replacement, and of the above planning and engineering issues is merited. It is felt that Federal funds may be available to the City for further transportation and planning studies during the initial planning and design phase of the bridge replacement project.

16. Development district portals and boundaries with distinctive design elements and landscaping. Connections to German Village and the Brewery District should be enhanced. The Main/Rich connector should receive additional landscaping. Improved landscaping of the Interstate 70 and 71 corridors is essential to improving the perceived character not only of the Downtown South district, but also of the Downtown as a whole. Interstate exits at East Main, South Front, and South Fourth streets offer ample opportunities for “gateway” landscaping treatments.

17. Encourage architectural design compatible with historic buildings in the district such as the Cultural Arts Center, the Southern Hotel, churches, and the Town/Franklin historic district. Brick should be the primary building material. Design review standards should be established for South High Street and East Main Street from High to Fourth Streets.

18. From High Street to the Scioto River, orient buildings and pedestrian corridors to maintain views of

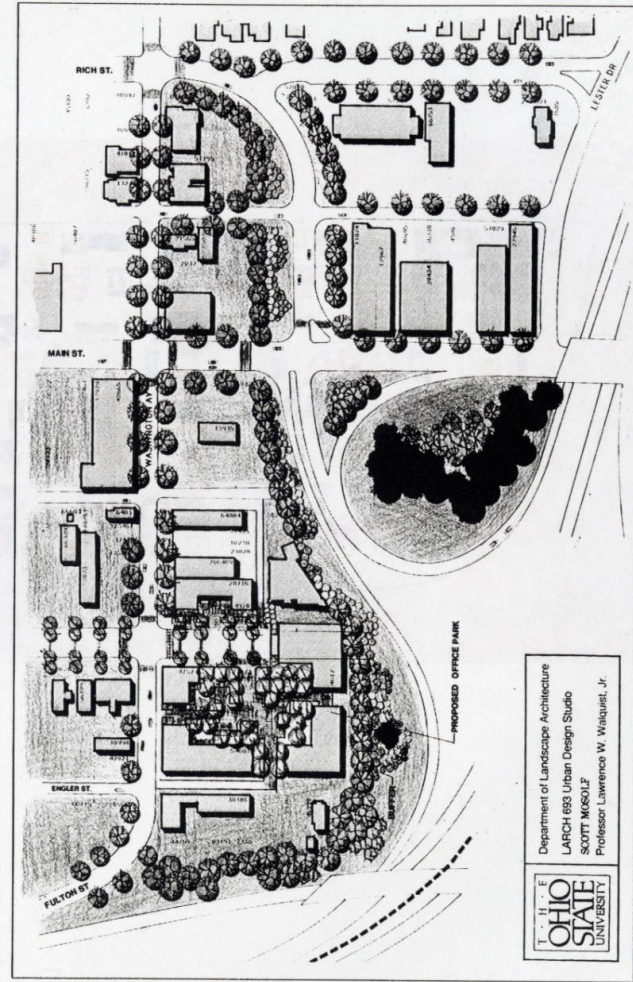
the riverfront. Building design in this area should acknowledge the special nature of the riverfront as a major public amenity.

19. In the area south of East Main and east of Grant Avenue, opportunities for mid-rise, multi-unit housing should be developed. business park office development, called for in the Discovery District Plan, should not preclude, by scale and character, such residential development opportunities.

20. Infrastructure improvements such as removal of overhead utility lines and installation of new lighting should be pursued as development continues. The majority of the sewer infrastructure consists of old combined sewers. Complete separation of sewerage flows and rehabilitation of the existing system should be pursued as development continues.

21. Institute cooperative voluntary agreements among businesses and property owners to landscape and maintain public areas and to accomplish other improvements and services in the Downtown South district. At the option of property owners, a Special Assessment District may be formed to provide such services or improvements. Services may include private security arrangements.

Main/Rich Connector at I-71: Development and Landscaping Opportunities



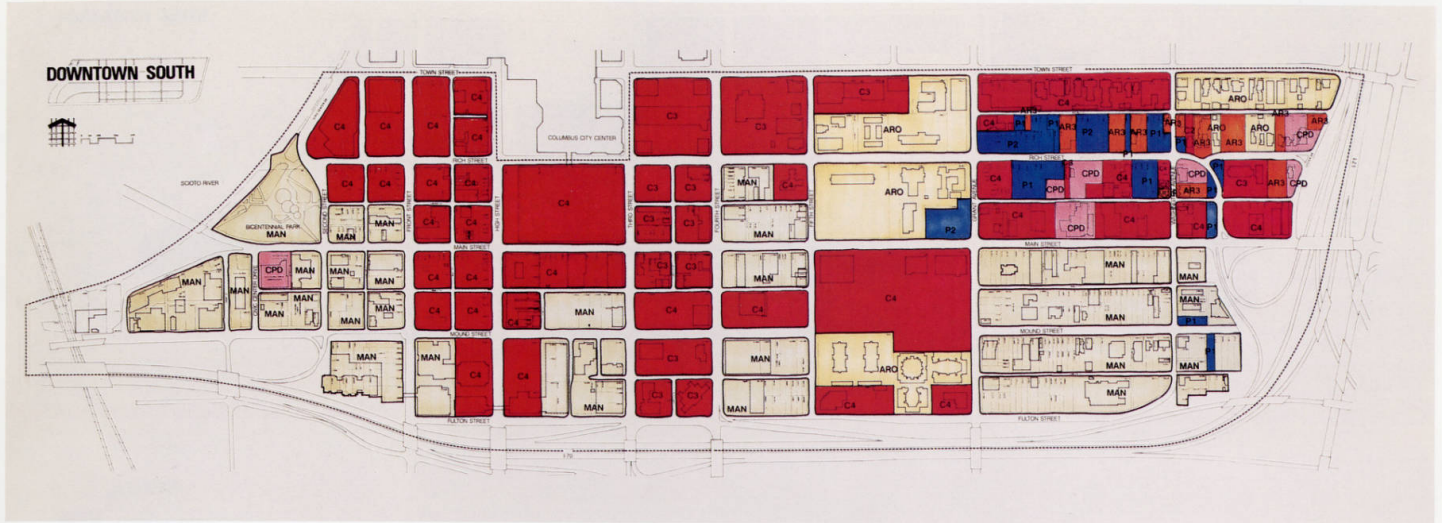
EXISTING LAND USE



PROPOSED LAND USE

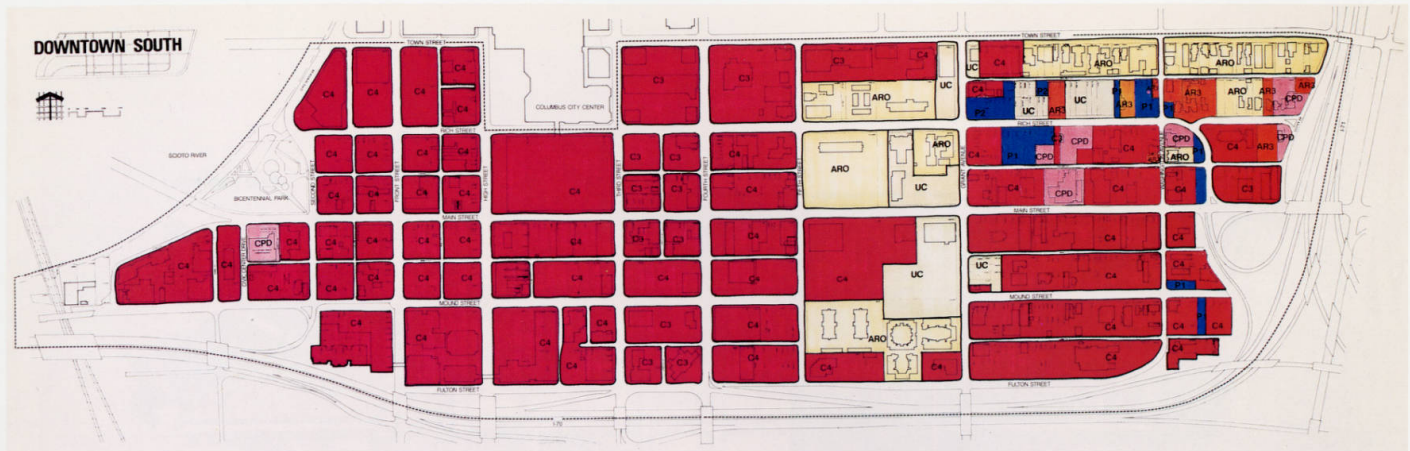


EXISTING ZONING



Existing zoning in the area reflects uses existing prior to 1950 and the major development trends and redevelopment projects of the last forty years. Up to the 1950's, manufacturing uses dominated the southeast and southwest quadrants of the district. This is reflected in the outdated zoning still in place. Of the 700 land parcels now contained in the Downtown South area, 347 are still zoned for manufacturing uses. However, fewer than 40 parcels still support uses that can be classified as manufacturing.

PROPOSED ZONING



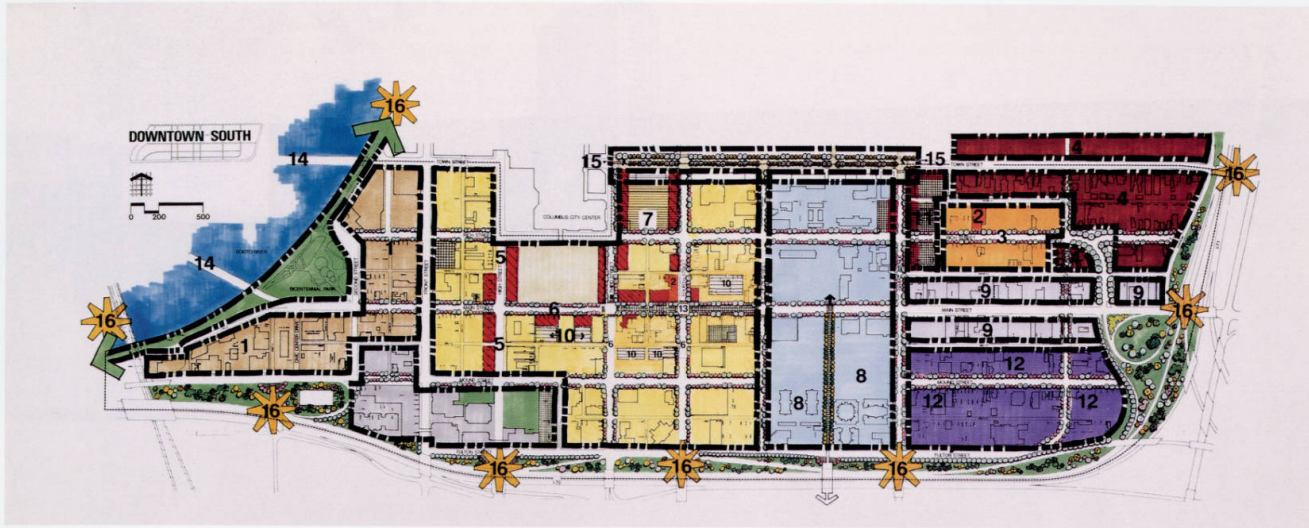
LEGEND

	Commercial (C2, C3, C4)
	Residential (AR3)
	Residential (ARO)
	Parking (P1, P2)
	Urban Campus - Future Rezoning (UC)
	Commercial Planned Development (CPD)

Two principles apply to proposed rezoning for the Downtown South District: (1) in no instance should proposed zoning be less restrictive than current zoning; and (2) proposed rezoning for the area should bring current and future land uses into line with actual development trends over the last forty years. Franklin University, the Columbus College of Art and Design, and Columbus State Community College are cooperating to propose an Urban Campus district which would allow for the planned integration of uses typically found on urban campuses. The proposed zoning shown here sets aside land owned by Franklin University for inclusion in this new zoning category. Manufacturing uses now current in the district would remain as non-conforming uses under the provisions of Chapter 3391 of Columbus City Codes.

Rezoning for the Downtown South District should allow for as much flexibility as possible to encourage a diverse mix of uses, facilities, and amenities throughout the district. Under the current zoning code, the category allowing for the most diverse mix of uses in a downtown area is C4 - Commercial.

MASTER DEVELOPMENT PLAN



PRIVATE DEVELOPMENT OPPORTUNITIES

- 1** Medium to High Density Housing and Supporting Retail
- 2** 'Loft Style' Residential Conversion
- 3** Neighborhood Scale Residential/Commercial
- 4** Low Density Residential/Office Rehab and Infill
- 5** Streetfront Retail with Mid to High Rise Offices Above
- 6** Commercial Infill (Compatible with Existing)
- 7** Greyhound Site Redevelopment (Mid Rise Retail and Offices)
- 8** 'Campus Style' Commercial/Residential Infill

- 9** Auto Oriented Retail
- 10** Alcove Parking
- 11** Parking Structure
- 12** Mid Rise Residential/Office Park Planned Development

PUBLIC/PRIVATE COOPERATIVE EFFORT

- 13** Main Street Demonstration Area

PUBLIC IMPROVEMENTS

- 14** Scioto River Bridges
 - Maintain Alignments
 - Pedestrian Amenities
- 15** Town Street Center Island
- 16** Portal Identification/Landscaping

Department of Landscape Architecture
LARCH 693 Urban Design Studio
Professor Lawrence W. Waagust, Jr.



BICENTENNIAL PARK

Showing Riverfront Housing

The area facing the riverfront and Bicentennial Park should be developed as multi-family housing, which can take advantage of the public recreational space and riverfront views. Careful design and judicious use of height limitations should be used to maximize this scenic opportunity. Convenience shopping and neighborhood cafes can be provided as residential amenities.



TOWN STREET CORRIDOR

Showing Greyhound Site

The wide Town Street right-of-way allows the creation of a landscaped median which will make the northern boundary of Downtown South into a distinctive urban corridor, with no loss of traffic lanes. This median will connect Grant Hospital and Franklin University with the City Center Mall. Along this corridor, the Greyhound Terminal site is a major development opportunity.



MAIN STREET DEMONSTRATION PROJECT

Showing Commercial Infill

Once a primary center of commercial activity, Main Street today has potential as a revitalized office, entertainment, and shopping street. The public walks should receive street trees, brick accents, and pedestrian-scale lighting and street furnishings. Landscaped parking “alcoves” and commercial infill structures should replace current surface parking.

CONCLUSION

The Downtown South Plan is meant as a guide, not a directive, for future development in the district. As it is not possible for planners to anticipate all future opportunities which may arise, each new idea must be evaluated against the goals and intentions of this plan. Over a period of time guided development will enrich the commercial, visual and social fabric of Downtown South.

The overriding priorities of the Downtown South Plan are simple. Exciting housing opportunities have already begun to take hold around the riverfront and Bicentennial Park, and these should be expanded. Retail and commercial infill opportunities, including opportunities for large-scale development, exist on now-vacant land near the central business district. Further east, commercial opportunities may be more auto-oriented, fulfilling a realistic need, while design and streetscape can reflect urban, rather than suburban, standards. In the Franklin University area, the formation of an "urban campus" environment should continue, replacing surface parking with multi-use and multi-level development. And finally, the historic properties in the district, including the Town/Franklin area, should be preserved.

It is quite possible to achieve the goals laid out by this plan. Downtown South has many assets which will aid in the implementation process. Foremost among these assets is the Downtown South Association, which is an established, active organization dedicated to improving the area. Downtown South also has a history

of successful redevelopment, as exemplified by the now completed Market Mohawk and Capitol South redevelopment projects. The partnership of the Downtown South Association with the City government and other entities in the area will help assure implementation of this plan.

When implemented, the Downtown South Plan will encourage the development of a vibrant and richly textured district of homes, businesses, shops and cultural facilities. With a character as diverse as the Riverfront and Town/Franklin historic districts, and as varied as the high-rise structures of South High Street from the modern campus buildings along Grant Avenue, Downtown South will be unique in its mix of uses, yet ordered in its growth and development. Because of this exciting and unique mix of opportunities, Downtown South has the potential for becoming a truly urban downtown district.

APPENDIX

DEVELOPMENT POLICIES AND STRATEGIES FOR DOWNTOWN SOUTH

Note: The following development policies and strategies were recommended by participants in meetings of community groups and community leaders in the Downtown South district during the planning process. These recommendations are consistent with the recommendations for Design and Development Opportunities contained in the Plan (page 11); in fact, in most cases, the Plan recommendations were derived from them. These policies and strategies should be considered a more specific elaboration of the recommendations contained in the Plan.

1) PHYSICAL IMPROVEMENTS/INFRASTRUCTURE

1.1) Bridges

- The Whittier Street Bridge should be rebuilt as an important connection spanning I-70 and linking the Brewery District and the Whittier Street riverfront area to DTS.
- The intersection of Ludlow and Town Streets could be improved to provide better merging of traffic before it crosses the Town Street Bridge.
- Further planning and engineering analyses are required to determine whether or not a rebuilt Town Street Bridge should remain on its current axis, or should be rebuilt to connect to Rich Street on the east bank of the Scioto.
- Improve pedestrian connection across Town and Main Street bridges.

- The RiverPlace development area (West Main to West Rich Streets, from Second to South Front Streets) should have well-designed, safe pedestrian connections across Civic Center Drive to Bicentennial Park. Similarly, there should be good pedestrian connections across Mound Street to Bicentennial Park.
- Based on pedestrian traffic patterns to German Village and the Brewery District, consider construction of additional pedestrian footbridges from Downtown South across I-70.
- The existing footbridge over I-70 at Sixth Street should receive design improvements and additional plantings.

1.2) Parks/Public Areas

- Develop mini-parks along pedestrian corridors.

1.3) Landscape

- Tie in landscaping improvements with 1992 Celebration.
- Improve visual links along major arterial corridors to tie the Downtown South District together. Methods may include planting flowering trees of a consistent type; banners along major corridors; and pedestrian lighting. Major corridors include East and West Main Streets; East Town Street; South Third and Fourth Streets; East and West Rich Streets; and South Front Street.
- A lineal green belt along the edges of the DTS District should be developed along I-70 and I-71. (Landscaping of these corridors with appropriate

trees, clearance of inappropriate trees and weeds, and a regular maintenance program to maintain trees will do much to improve the overall image of downtown.)

1.4) Streetscape

- Improve visual links along major corridors to tie district together (see 1.3, above).
- Improve street-level continuity. In addition to landscaping (see 1.3, above), street-level architectural continuity should be preserved for retail and commercial purposes; setbacks in designated commercial districts should be discouraged; building frontages should be brought up to the sidewalk; and retail space at ground-floor levels should have direct access to sidewalks.
- On some streets, sidewalks may be widened or street landscaping features added without impeding traffic flow (see Master Development Plan, Key 15).
- Develop streetscape design criteria. In most cases, design criteria suggested in the Discovery District streetscape plan will be appropriate.
- Design and develop gateway entrances to Downtown South. West Main Street crossing the Scioto River into downtown; South Fourth Street at Fulton; South Grant Avenue at Fulton; the Main/Rich Interconnect from I-71; and South Front Street at Fulton offer opportunities for gateway treatments.
- In certain limited areas, including those recommended for campus-style commercial or

residential development, create larger setbacks along building fronts for green, active space, allowing additional height in return.

- Improve lighting along walkways and corridors from Downtown South to German Village to encourage pedestrian movement.
- Improved pedestrian lighting should be an essential part of streetscape improvements throughout the district.
- Alleys as well as streets should be better lit. An alley lighting program should be carried out with the cooperation and participation of building owners along alleys.
- Develop lighting standards and criteria. This should be done as part of the overall Lighting Plan for downtown.
- Plan for placement of trash receptacles as part of streetscape improvements in high-volume pedestrian areas.

1.5) Utilities

- Where possible, new utility lines should be located underground. In areas specified for pedestrian and retail improvements, existing overhead lines should be located underground, in cooperation with the utility companies.
- As development occurs, each site will be required to separate sanitary and storm sewerage flows. The overall design and planning of separate sewer systems will be coordinated by the Division of Sewerage and Drainage.

2) DEVELOPMENT

- A major objective of development policy should be to re-create and fill in major pedestrian/street oriented axes through Downtown South: East Main Street, South High Street, South Third and South Fourth Streets, and South Front Street. (East Town Street is already fairly continuous from South Third to Grant Avenue).
- Infill, street-oriented retail development should occur on East Main Street between High and Fourth; on South High between Town and Mound; on South Third between Town and Main; and on South Fourth between Town and Main.

2.1) Mixed-Use

- Encourage street-level commercial, retail, and entertainment uses to give the Downtown South District a pedestrian orientation.
- Create opportunities for retail and service businesses to serve residential development.

2.2) Retail/Restaurant

- Fill existing retail vacancies.
- Create opportunities for retail and service businesses to serve residential development, in areas where residential development is both desirable and feasible.

2.3) Housing

- Encourage housing renovation.
- Identify best opportunities for student housing to serve the development of downtown educational

institutions.

- Create residential opportunities for hospital employees.
- The Ironsides site should be developed as a residential complex with supporting retail uses.
- The Lazarus Annex site at Town and Front Streets offers an opportunity for residential development with supporting retail.
- The block bounded by West Main and West Mound Streets, from Front to High Streets, offers opportunities for mixed use development to include commercial office space; with ground-level retail on High Street; and residential on the western edge.

2.4) Development Scale and Appropriateness

- The appropriate mix of uses for the southeast corner of the Downtown South District is a mix of commercial and residential uses, in campus-like settings.

2.5) Development Opportunities

- City Center frontage on South High and East Rich Streets should be developed as street-oriented retail, especially in the former Centrum area at High and Rich, and should be utilized as a development focus for the area.
- The Ironsides site should be developed as a residential complex with supporting retail uses. Design should step back from the river and should maintain pedestrian access along the river.
- South Front Street should develop as a corridor of

parking and other ancillary uses serving both High Street commercial buildings and the riverfront corridor.

- Residential development opportunities in the area south of East Main Street, between South High and Fourth Streets, should be pursued.
- In the area south of East Main Street and east of Grant Avenue, opportunities for mid-rise, multi-unit housing should be developed.
- The Greyhound Terminal site should be utilized as a major development opportunity, potentially for a mix of retail and office uses.
- East Main Street from South High Street to South Fifth should be a targeted area for retail/commercial infill and streetscape improvements.
- Relocate historic older homes and build compatible infill housing/office structures into existing gaps on Rich Street.

2.6) Development Finance

- Determine priorities for capital improvements: i.e. streetscape improvements, including pedestrian lighting, tree plantings, and signage; bridges and gateway improvements; parks and public areas; etc.
- Seek capital funding to improve and landscape major Downtown South arterial corridors.

3) QUALITY OF LIFE

3.1) Arts/Cultural Development

- Create additional small performance areas for music and performing arts as part of a complete “downtown experience”.
- Continue and develop linkages between recreational space and cultural (arts/entertainment) uses. This will involve development of more and better outdoor programming for the arts.
- Develop pedestrian connections from cultural attractions (on Capitol Square and East Main Street) south to Brewery District, and westward to the river park system. This would involve making South Front and South High Streets better places to walk; making pedestrian improvements on West State; West Main; West Rich; and West Mound Streets, including tree plantings and sidewalk repairs.
- Retain and develop historic facilities. Among the historic buildings in the area, the restoration of the Southern Theater stands out as the principal opportunity to create a cultural/entertainment anchor not only for East Main Street development but also for the entire Downtown South district.
- Downtown South can provide a home for artistic and cultural institutions, as well as for housing and studio space for artists. A possible location for these may be found in the upper floors of existing buildings in the district.
- Well-lighted and safe connections to restaurants

and parking are an important part of cultural development.

3.2) Preservation/History

- Encourage re-use of existing buildings. Identify those buildings with potential re-use value, and work with owners and prospective developers to encourage specific projects.
- Existing review of demolition projects is intended to create opportunities for redevelopment.
- Nominate specific buildings within the area (see Historic Structures Map) for inclusion on the Columbus Register of Historic Places.

3.3) Day Care

- Identify specific opportunities for and encourage provision of day care (both child and elder care), accessible to businesses, institutions, and employees of the Downtown South district.

4) MANAGEMENT/ADMINISTRATION

4.1) Image/Marketing

- Image-building efforts for Downtown South area should emphasize convenience and walkability of the district.
- Streetscape improvements on Main Street east to South Fifth and along other arterial corridors are the most important from a public perception standpoint. These areas need immediate attention from a visual, lighting, and safety standpoint.

4.2) Maintenance

- Utilize Franklin County's prisoner work-release program to help clean streetscape.
- Plan for placement of trash receptacles as part of streetscape improvements in high-volume pedestrian areas (see 1.4, above).

4.3) Safety

- Install emergency or service phones like those in use on the OSU campus.
- Institute efforts with the Public Safety Department and with private property owners to control panhandling and vagrancy in areas of South High, Main, and Noble Streets. This may include a review of wine, beer, and liquor sales and licensing within the District.
- Alleys as well as streets should be better lighted.
- Increase police patrols on streets and alleys.

4.4) Code Enforcement

- Work with the City's Division of Development Regulation and with private property owners to assure that the quality of buildings, sidewalks, and other structures throughout the District is consistently maintained.

4.5) Funding

- At the option of property owners, institute a special assessment district, either a voluntary or a special improvement district (SID), in Downtown South for purposes of capital improvements, maintenance, safety, and promotion.

4.6) Implementation

- Create an oversight mechanism to ensure that proposals of both the Strategic Plan and the Downtown South Land Use Plan are carried out.

5) TRAFFIC/CIRCULATION

- A free-standing parking structure accessible to the public should be constructed at or near the corner of Rich and Grant Streets. This structure should incorporate street-level amenities, including retail, if possible.

5.1) Public Transportation

- A free shuttle system should link Downtown South to other districts, including the Discovery District, and should tie the Downtown South District together. Shuttle service should include access to parking areas on the periphery of the District.
- Shuttle service should link cultural/arts areas to other destinations and to remote parking. Such a service would extend and improve that recommended in the Discovery District plan.
- Review bus stop locations to assure that high-volume pedestrian areas are served.

5.2) Pedestrian Circulation

- Mid-block crosswalks should be developed to provide better access across streets.
- Pedestrian amenities should be added to the Sixth Street mallway, extending an improvement program already begun by Franklin University.
- Landscaped pedestrian connections should be

developed from the Downtown core to Bicentennial Park. These connections should be developed from West Town and High Street intersections to Bicentennial Park and from West Mound and High Street intersections to Bicentennial Park.

- A walking tour through the Downtown South District should connect to adjacent neighborhoods both east and south.
- Enhance pedestrian connections to German Village and Brewery District.

5.3) Vehicle Circulation

- Major arterial corridors through Downtown South should not interrupt pedestrian links between/ among related uses.
- Street and traffic improvements should seek to optimize the mix of vehicular and pedestrian traffic through the district, and to encourage public transit where feasible.
- Further planning should specify which areas should receive priority for pedestrian, and which for vehicular traffic. An important goal should be to facilitate peak traffic flow, but not always to increase vehicular speeds.

5.4) Parking

- Design of parking structures in the commercial core of the Downtown South District and within pedestrian areas should emphasize street-level pedestrian amenities, including retail, which should be included where feasible.

- Multi-level municipal parking should be developed in the area of South Third and East Mound to support business development in the area.
- Parking should be developed to serve multiple needs, especially near the core business district.
- Plan and design appropriately located parking garages to serve the entire district. Private development of a parking structure between Fourth and Fifth Streets on East Main is appropriate.
- On-street parking for residents and businesses may potentially be addressed by special permit, especially in the Rich and Town Street corridors. Maximum meter limits should be reviewed in multi-use areas such as the Town/Franklin neighborhood to discourage all-day meter parking by downtown employees.
- Parking lot appearance should be enhanced with major planting programs and appropriate enforcement of parking sections of the Columbus City Code. Special permits for use of undeveloped sites for parking should be limited to a two-year period and closely scrutinized for renewal.
- The Downtown Parking Exempt District should be expanded eastward to I-71, depending on the result of meetings to be held with businesses and property owners early in 1992.
- As development occurs, parking should be consolidated into a few multi-level garages, allowing former surface lots to be developed or used as green space.
- Street parking on South Grant Avenue and other

arterials should be reviewed, with the intent of allowing residential parking between and after peak traffic hours.

- Future institutional planning in the Downtown South and Discovery Districts should identify sites for new parking structures, especially for combined use of several large institutions. Identify the areas in which additional parking demands are likely to create stresses, especially in the Grant Avenue corridor to serve needs of Grant Hospital, Franklin University, and other commercial development in the vicinity.

6) LAND USE PATTERNS

- The area south and west of the Town/Franklin neighborhood should provide parking and other support uses both for the neighborhood, for neighborhood commercial, and for the institutions west of Grant Avenue. (Where appropriate, parking should be consolidated into structures: see 5.4, above) (see Master Development Plan, Key 2).
- Main Street east of Grant Avenue should further develop its current auto-oriented commercial character. However, the design emphasis should be urban, rather than suburban, meaning that new development setbacks should either be zero, or should provide only for pedestrian amenities such as landscaping. (see Master Development Plan, Key 9).
- The Ironsides site should be developed as a residential complex with supporting retail uses. (see Master Development Plan, Key 1).

- East of Bicentennial Park, mixed-use residential and commercial development is appropriate. Residential units should orient toward the river, with commercial development “stepped back” behind and above residential space. (see Master Development Plan, Key 1).
- The area bordered by South Front Street to the west and South Fifth Street to the east is appropriate for mixed uses of either medium to high-rise scale. This is part of the downtown commercial core, and should reflect the scale and architectural standards of the Capitol Square District. (see Master Development Plan, Key 6).
- South High Street building frontages should reinforce the retail character of the corridor at street level. (see Master Development Plan, Key 5).
- Building frontages on East Main Street between South High and Fifth should reinforce the retail character of the corridor at street level and should emphasize pedestrian scale and amenities. (see Master Development Plan, Key 13).
- South Front Street should develop as a corridor of parking and other ancillary uses serving both High Street commercial buildings and the riverfront corridor. However, good design features, pedestrian scale, and pedestrian amenities should serve to enhance pedestrian access both along South Front Street and to uses both east and west along Mound, Main, and Rich Streets. Front Street should not look like an alley.
- Infill street-oriented retail development should occur on East Main Street between High and Fourth; on South High Street between Town and Mound; on South Third Street between Town and Main; and on South Fourth Street between Town and Main. (see Master Development Plan, Key 5).
- The Market-Mohawk Urban Renewal area between Fifth and Grant Streets has developed a campus-like character for commercial, residential, and educational uses which should be maintained. (see Master Development Plan, Key 8).
- Town Street between Third and Grant has developed a mid-rise commercial and institutional character with landscaped setbacks, which should be maintained. Tree plantings and other pedestrian-friendly features should be emphasized along Town Street. (see Master Development Plan, Key 15).
- Main Street between Third and Fifth Streets should be targeted for commercial rehabilitation and infill (see Master Development Plan, Key 13).
- Residential development opportunities in the area south of East Main Street, between South High and Fourth Streets, should be pursued.
- The Town/Franklin Historic District should preserve its historic and residential character, allowing for compatible office or institutional uses. Infill development should be consistent, both in scale and design, with existing building stock (see Master Development Plan, Key 4).
- Streetscape and other public amenities to enhance the historic, residential character of the Town/Franklin neighborhood should be encouraged (see Master Development Plan, Key 4).

- Residential or commercial rehabilitation and infill projects in the Town/Franklin Neighborhood should maintain residential scale and character (see Master Development Plan, Key 4).
- In the area south of East Main Street and east of Grant Avenue, opportunities for mid-rise, multi-unit housing should be developed. Business park office development, called for in the Discovery District Plan, should not preclude, by scale and character, such residential development opportunities (see Master Development Plan, Key 12).
- As called for in the Discovery District Plan, both Washington Street and Grant Avenue should be continuously developed with building and streetscape features to encourage pedestrian use.

7) DESIGN POLICIES

7.1) Building Design

- New building design on Main Street should encourage pedestrian access and circulation by orienting facades to the street, with minimal setbacks.
- Building design in the Scioto riverfront area should be “stepped back” from the river’s edge to afford maximum riverfront views from developments oriented toward the river corridor, as called for in the Riverfront Strategic Plan (1985).
- Building frontages on East Main Street between South High and Fifth should reinforce the retail character of the corridor at street level and should

- emphasize pedestrian scale and amenities.
- South High Street building frontages should reinforce the retail character of the corridor at street level.
- South High Street should develop as a high-density corridor.
- Infill development in the Town/Franklin neighborhood should be consistent, both in scale and design, with existing building stock.
- In the area south of East Main Street and east of Grant Avenue, business park office development, called for in the Discovery District Plan, should not preclude, by scale and character, residential development opportunities.
- In the core business area from High Street to Fifth Street, paint should be cleaned off brick. Infill should be in keeping with older significant buildings. Good design features should help to improve the present character.
- Especially along Main Street, expedite and improve the existing review process for buildings, signs, graphics, landscaping.

7.2) Streetscape Design

- An automobile-oriented character to Main Street (east of Grant Avenue) should be encouraged as long as designs enhance urban, rather than suburban, character.
- Parking and other commercial support uses in the area south and west of the Town/Franklin neighborhood should be “softened” by landscaping and pedestrian-scale design elements to encourage

- pedestrian circulation through this area.
- South High Street should develop as a high-density corridor.
 - Main Street from Bicentennial Park to South Fifth Street should be lined with trees along sidewalks. Parking in this area should be screened by plantings.
 - In the core business area from High Street to Fifth Street, good design in graphics and storefronts should be encouraged.
 - Good design features, pedestrian scale, and pedestrian amenities should serve to enhance pedestrian access along South Front Street and to uses both east and west of South Front along Mound, Main, and Rich Streets. Front Street should not look like an alley.
 - Tree plantings and other streetscape features should be emphasized along South High, South Front, and East Town Streets.
 - Park-like corridors from the commercial core of Downtown South to the Scioto River should be extended along the following streets: West Town, West Rich, and West Main.
 - Along East Main Street, from South High to South Fifth, “pocket” parks should be encouraged in commercial areas to recall the character of the “institutional” portion of Main Street from Fifth to Grant.
 - New lighting standards should follow current High Street design standards and should become a catalyst for Downtown South development if instituted early and given high priority for funding.

- On Fulton Street, move parking from south to north side of street and interrupt parking at intervals with tree islands.
- On Fulton Street, improve planting at freeway fences and add more appropriate trees along freeway. In general, plantings along freeway corridors should be improved and better maintained (see 1.3 and 1.4, above).
- The Riverfront development area should have pedestrian plaza connections over Civic Center Drive to Bicentennial Park, as well as over Mound Street to Bicentennial Park.
- The existing footbridge over I-70 should receive design improvements and additional plantings. Additional pedestrian connections across the freeways should receive attention in future development efforts.

7.3) Gateway Concepts

- All major arterial and freeway corridor entries to Downtown South should be developed as significant and beautiful portals. (This would implement a concept recommended as long ago as the 1973-74 Ponte Plan for downtown).